

# 1 Introduction and scope of the Plan

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## 1.10 Area Wide Issues

The essential elements of a sustainable city are the residential areas, the distribution of key activities, such as retail, recreation, community services and facilities plus the transport networks linking them together. Natural resources are important in the sustainable city.

In this section, area wide issues are identified and discussed with adoption of policies and strategies.

### 1.10.1 Resource Management and the Tangata Whenua of Lower Hutt

Issue

**It is important that the Plan recognises the relationship between tangata whenua and wider resource management issues in the City. Sections 6(e), 7(a) and 8 of the Act require the relationship of Maori and their ancestral lands to be recognised and provided for as a matter of national importance, and for those exercising functions and powers under the Act, to have particular regard to kaitiakitanga and to take into account the principles of the Treaty of Waitangi.**

**Kaitiakitanga encompasses the Maori view of guardianship, involving the spiritual dimension as well as the physical dimension. The onus on people who say they are kaitiaki is to acknowledge all the responsibilities that come with kaitiakitanga, including actively looking after and caring for their mana whenua and taonga. Kaitiaki are required to continually be part of the process of environmental decision-making.**

**Kaitiakitanga includes an obligation on people to use resources in ways that respect and preserve resources in the environment, both physically and as sources of spiritual power. The tangata whenua who have mana over resources are able to determine both the characteristics of kaitiakitanga and how it should be given expression.**

Objective

**To respond to the principles of the Treaty of Waitangi and other matters of significance to the tangata whenua as specified in the Act.**

Policies

- (a)** To have particular regard to tangata whenua's desire to carry out kaitiakitanga.
- (b)** To protect waahi tapu and sites of cultural or historical significance to tangata whenua from desecration or disturbance.

- (c) To recognise and protect the tangata whenua desire to maintain and enhance their traditional relationship with the environment.
- (d) To consult with the tangata whenua when discharging functions and duties under the Act.

#### Explanation and Reasons

Chapter 2 provides a more comprehensive explanation of the partnership between tangata whenua and resource management in Lower Hutt.

## 1.10.2 Amenity Values

#### Issue

**The different character and amenity values of areas contribute significantly to the environment of the City. The Act recognises the importance of people's environment (which is defined to include amenity values) and it is necessary to recognise these as essential elements in the Plan.**

#### Objective

To identify, maintain and enhance the character and amenity values of the different activity areas.

#### Policy

To identify within all activity areas the general character and amenity values of that activity area.

#### Explanation and Reasons

### **Residential Activity Areas**

#### **General Residential Activity Area:**

This Activity Area accounts for much of the residential development in the City. It is dominated by single dwellings on fee simple subdivisions, but also contains a variety of other housing styles including cross lease developments, semi detached housing, and a limited number of multi-unit developments. Semi detached and multi-unit developments are more common in some locations than others. Generally sites within the Activity Area have a flat topography, this being a natural feature or being a result of earthworks during the development stages. Sites in most locations have been well developed with maturing domestic scale landscaping and planting. While small scale businesses, which can be classed as home occupations, are common there are few larger commercial or industrial activities.

#### **Special Residential Activity Area:**

Three locations, Woburn, the Military Road area, and Lowry Bay, have been identified where sites are characterised by lower density development on larger sites often with mature landscaping and planting.

#### **Historic Residential Activity Area:**

Patrick Street, Petone: This historic area was largely developed at the time of the 1905 Workers Dwellings Act when the first state housing schemes were built. A variety of designs were used, comprising both single and two storey dwellings. Appendix Historic Residential 1 details the specific elements that contribute to the character and amenity values of this historic area.

Riddlers Crescent: This historic area was largely developed between 1906 and 1910 and consists of both villa designs and semi-detached workers' dwellings. Appendix Historic Residential 1 details the specific elements that contribute to the character and amenity values of this historic area.

#### **Hill Residential Activity Area:**

This Activity Area consists of significant amounts of land in the hillier parts of the City. The topography of these areas is such that individual sites have characteristics of slope, are often above or below road level, have a different relationship with neighbouring sites to those on the flat, and have views.

#### **Landscape Protection Residential Activity Area:**

This Activity Area is characterised by particularly steep sites with large land areas. The amenity values are influenced by this topography, vegetation cover and the potential impact of development including the creation of driveways and building platforms.

### **Commercial Activity Areas**

#### **Hutt City Central Commercial Activity Area:**

This Activity Area is the central focal point of the city as the main area of commercial, community and civic activities. Further diversity in the activity mix is anticipated, with increased levels of residential activities and service industries. The environment is characterised by a number of complementary activities of different size and scale. Buildings are of a mix of heights and ages, are constructed in a variety of styles and with a diverse range of materials. The relationship of buildings to the public realm (streets and open space areas) significantly contributes to the amenity values of the Central Area. Large surface areas of carparking and car sales could detract from the amenity values in this area. Improvements to the amenity values in the central area are planned, including improving the building quality and public realm. New private development or significant redevelopments are expected to contribute to such amenity values, while still remaining commercially workable or viable.

#### **Petone Commercial Activity Area:**

Area 1 – Jackson Street between Victoria and Cuba Streets: This part of Jackson Street is dominated by one and two storey buildings built between 1926 and 1940. The subdivision pattern is of small allotments with narrow frontages. Retail and commercial activities occur at ground level and commercial and residential uses above ground level. The close proximity of a residential activity area immediately behind the narrow band of commercial properties contributes to the character and amenity values. The character and amenity values of this area are strongly influenced by the heritage values of the buildings. Appendix Petone Commercial 1 details the specific elements that contribute to the character and amenity values of this historic area.

Amendment 1

Area 2 – Area generally bounded by Hutt Road, Petone Avenue, Campbell Terrace, Victoria Street, Sydney Street and The Esplanade: This part of Petone is currently characterised by a range of retail, commercial and industrial activities. It is intended that this area is to be transformed into an attractive and vibrant mixed use area, with higher amenity levels than present. This area is intended to accommodate a wide mix of activities including residential, commercial, large format retail, community and some light industrial and service activities.

The future character of the area is of attractive entrance gateway routes into the City and buildings, structures and associated areas which are functional, attractive and contribute to the quality of the environment.

The character and amenity values in this area are influenced by the more open nature of sites, a diversity of building scale, the coastal environment for those sites fronting The Esplanade, and mixed land uses.

**Suburban Commercial Activity Area:**

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# 3 Definitions

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**100-Year Flood:.....**

**Integrated Retail Developments:**

Amendment 2

an individual retail development, or a collection of any two or more retail activities that are developed and operate as a coherent entity (whether or not the activities are located on separate legal titles), and share one or more of the following:

- (a) servicing and/or loading facilities;
- (b) vehicle and/or pedestrian access;
- (c) car parking;
- (d) public spaces and/or facilities.

This definition includes shopping malls and large-format retail parks, but does not include trade supply retail, wholesale retail, yard-based retail or building improvement centres.

**Intensive Farming:.....**

# 5 Commercial

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## Introduction

Commercial centres around the City provide residents with the ability to obtain the goods and services they require to meet their needs. To recognise the distinct differences between the commercial centres in the City, such centres have been categorised into the following four activity areas:

- (a) Central Commercial;**
- (b) Petone Commercial;**
- (c) Suburban Commercial; and**
- (d) Special Commercial.**

In this Plan retailing is generally confined to the above four areas. In order to ensure that these commercial centres become strong focal points in the community, it is important that retailing be restricted in non-commercial activity areas. It is considered that such an approach will ensure that a sense of place can be achieved in commercial centres. In addition, existing public infrastructure and services associated with commercial centres will be used efficiently. Grouping retail activities enables public transport to be provided to such centres, and this will result in a more efficient use of a non-renewable energy resource.

### (a) Hutt City Central Commercial Activity Area

The Hutt City Central Commercial Activity Area is one of the largest in the Wellington region. Over the last few years there has been considerable commercial and retail growth. It is important that this growth and development be accommodated so that a healthy, vibrant and vital commercial centre can be achieved.

The Hutt City Central Commercial Activity Area is that area generally bounded by High Street to the south, Cornwall Street to the east, Daly and Rutherford Streets to the west, and Melling and Brunswick Streets to the north, including the Market Grove area.

### (b) Petone Commercial Activity Area

The Petone Commercial Activity Area is strategically positioned at the hub of the Wellington region. The centre has a wide catchment area, and caters for a significant number of shoppers that come from beyond the immediate area for a variety of reasons. Two distinct areas are identified in the Plan as follows:

#### (i) Area 1 - Victoria Street to Cuba Street

Area 1 consists of both sides of Jackson Street, between Victoria and Cuba Streets, and adjoins residential activity areas. These commercial sites are generally small and consist of a mix of one and two storeyed buildings, with small frontage retail and commercial activities at road level. Residential flats and offices are located above street level activities. Many of the buildings in this area were built between 1926 and 1940, and have a distinctive built form, style and character. It is important that this be protected.

**(ii) Area 2 – Petone Mixed Use**

Area 2 comprises the area generally between Hutt Road, Petone Avenue, Campbell Terrace, Victoria Street, Sydney Street and The Esplanade illustrated in Appendix Petone Commercial 5. This area is suitable for mixed uses, providing a range of residential, large format retail, commercial activities and small-scale or low intensity light-industrial, business and service activities.

**(c) Suburban Commercial Activity Area**

The Suburban Commercial Activity Area provides residents with facilities to meet their day to day needs, and to engage in community activities in close proximity to their homes. This Activity Area includes all but a few very small suburban retail locations in the City. These small retail centres are incorporated in the residential activity area, where they are recognised as non-residential activities. By including retail shops in a residential activity area, those shops that are underutilised or remain empty for long periods of time, have the opportunity to become residential dwellings.

Suburban Commercial centres within the City vary in size and function. The larger centres such as Naenae, Stokes Valley and Wainuiomata, provide a wide range of activities, including supermarkets, banks and libraries, and serve a large catchment area. The smaller commercial centres such as Waiwhetu and Waterloo, provide more of a neighbourhood service, with facilities such as dairies, takeaway shops and hairdressers. While the Suburban Commercial Activity Area provides for a number of activities, there is an emphasis on minimising any adverse effects on residential activity areas.

**(d) Special Commercial Activity Area**

The Special Commercial Activity Area has two areas. They are as follows:

**(i) Area 1 - Station Village**

Area 1 comprises the Station Village complex at the western end of Railway Avenue. Station Village was originally established as a tourist and entertainment centre, and consequently encourages activities of a similar nature. The total floor area provided for retailing activities in this Activity Area is purposely limited, to encourage the establishment of leisure, entertainment and craft facilities. In addition to these facilities, Station Village encompasses a recognised historic building, and additional buildings designed in keeping with the historic theme. While it is important to maintain a leisure, entertainment and craft centre for the City, activities and buildings will not compromise the historic significance of the Station Building and its complementary surroundings.

**(ii) Area 2 - Boulcott Village**

Area 2 consists of the Boulcott Village commercial centre on High Street. The close proximity of Boulcott Village to the residential area warrants the need to protect residential amenity values from the adverse effects of activities at the commercial centre.

## 5B Petone Commercial Activity Area

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### 5B 1.1 Local Area Issues

#### 5B 1.1.1 Area 1 - Area on Jackson Street generally between Victoria and Cuba Streets

##### Issue

On both sides of Jackson Street between Victoria and Cuba Streets sites generally are small and adjoin residential activity areas. It is important that the scale and character of activities are controlled so that there are no encroachments into the adjoining residential areas and adverse effects, such as adverse traffic effects, are managed.

##### Objective

To ensure that activities in the area of Jackson Street generally between Victoria and Cuba Streets do not have adverse effects on adjoining residential activity areas.

##### Policy

- (a) To ensure that only small scale activities are permitted on Jackson Street generally between Victoria and Cuba Streets so that there is no likelihood of encroachment into adjoining residential activity areas and adverse effects, such as adverse traffic effects, are managed.

##### Explanation and Reasons

Sites on Jackson Street generally between Victoria and Cuba Streets, are small and adjoin residential activity areas. It is important that large scale and more vehicle oriented activities are excluded to ensure that adverse effects are minor and that there are no encroachments into adjoining residential activity areas.

#### 5B 1.1.2A Area 2 – Petone Mixed Use - Area Generally Bounded by Hutt Road, Petone Avenue, Campbell Terrace, Victoria Street, Sydney Street and The Esplanade - Activities

Amendment 4

##### Issue

Mixed uses which complement and support each other, such as commercial, small-scale or low intensity light-industrial, business and service activities, residential and large format retail activities in the western end of Petone would support the social and economic well-being of the area and the City as a whole. However, they could also detract from the established vibrancy and vitality of the Jackson Street Historic Retail Precinct and Hutt City's Central Commercial Activity Area. It is also necessary to manage the potential adverse effects, including noise, dust, odour, glare, light spill and traffic, of activities so as to maintain and enhance the quality of the environment. In addition, potential reverse sensitivity effects and incompatibility effects between activities may occur.



## Objective

To provide for a mixed use activity area within Petone which caters for a range of complementary commercial, small-scale or low intensity light-industrial, business and service activities, residential and large format retail activities, increasing the number of residents and workers in Petone, and avoiding or mitigating adverse effects so that amenity values and character of the area, neighbouring areas and the overall environment are maintained or enhanced.

## Policies

- (a)** Provide for a range of residential, commercial, small-scale or low intensity light-industrial, business and service activities, and large format retail activities, provided their effects are compatible with each other and the character and amenity values of the area.
- (b)** Manage larger scale retail activities to ensure they do not detract from the vibrancy and vitality of Jackson Street Historic Retail Precinct Petone Commercial Activity Area – Area 1) and Hutt City's Central Commercial Activity Area and create an attractive and public focused environment.
- (c)** Restrict smaller scale retail activities to ensure they do not detract from the vibrancy and vitality of Jackson Street Historic Retail Precinct (Petone Commercial Activity Area – Area 1) and Hutt City's Central Commercial Activity Area.
- (d)** Provide for residential activities which have quality living spaces for residents, meet the service needs of this type of activity, and adopt on-site measures to mitigate potential incompatibility issues with other activities.
- (e)** Restrict residential activities at ground floor level along Jackson Street, thereby retaining retail and commercial activities along the Jackson Street pedestrian focused frontage.
- (f)** Restrict activities, including heavy industrial or late-night activities, which may be incompatible with residential and other activities and/or degrade the character and amenity values of the Petone Mixed Use Area.
- (g)** Manage the establishment and operation of activities where traffic generation is likely to have adverse effects on the safety and efficiency of the transport network.
- (h)** Ensure that effects likely to be generated by each activity, such as noise, dust, odour and traffic, are managed to avoid or mitigate adverse effects on the amenity values and character of both the area and properties within the mixed use area and in nearby Residential Activity Areas.
- (i)** Ensure that effects likely to be generated by each activity are managed to avoid or mitigate any adverse effects causing harm or damage to the receiving environment.
- (j)** Manage the effects of development on the supply and demand of local infrastructure, including drainage infrastructure, to ensure that development is within capacity limits or the infrastructure capacity is upgraded.

## Explanation and Reasons

There is demand for an area within Petone to accommodate a range of complementary activities including residential, large format retail and commercial activities. Petone Commercial Activity Area - Area 2 is suitable as a mixed use area for the following reasons:

- (a) There are a wide range of sites, in terms of size, configuration and existing built development which can be used, adapted or redeveloped to accommodate a range of activities. These activities would serve both the local and wider community;
- (b) The area adjoins the small scale speciality retail area of Jackson Street Historic Retail Precinct (Petone Commercial Activity Area - Area 1) and thus a sense of place can be achieved which provides a range of complementary residential, retail and commercial activities;
- (c) The area is well situated in terms of the regional transportation network (including public transport) and other public and community infrastructure and services;
- (d) The area adjoins the Petone Foreshore which offers visual interest, open space and recreational opportunities.

Providing for a wide range of activities in the Petone Mixed Use Area gives landowners flexibility in the use of land and buildings to meet the changing dynamics of the economy and society. This flexibility would assist in creating and maintaining a vibrant and attractive mixed use area which supports and recognises the established activities and qualities in Petone. This provision of mixed use seeks to make efficient use of the land within the western end of Petone, providing opportunities for the re-use and redevelopment of existing buildings and properties for different activities.

For mixed use areas with a business and residential activity mix to work well, compatibility issues need to be managed. These issues arise between different forms of development (bulk/scale) and also in relation to the effects of certain activities (including noise, glare, odour and parking). Provisions for the mixed use area are designed to manage these issues, while recognising that a mixed use area offers a different type of amenity and lifestyle opportunity, than that found in predominantly residential or business areas.

In providing for a wide range of activities, there is potential to cause adverse effects both within the Petone Mixed Use Area and in areas beyond its boundary, such as nearby residential areas. These effects include dust, odour, noise and glare. The Plan manages these effects through applying performance standards to ensure these effects are avoided, remedied or mitigated.

Retail activities are continually changing in response to market pressures. The Petone Mixed Use Area has developed as a location for larger format retail activities. There is potential if a high number of smaller scale speciality or comparative shops develop in the Petone Mixed Use Area could degrade or undermine the vibrancy and vitality of the Jackson Street Historic Retail Precinct (Petone Commercial Activity Area - Area 1) which forms the core of the existing retail area in Petone and the Lower Hutt City central area. Therefore, a limitation is placed on the size of smaller and larger retail activities to maintain the role and economic, cultural and social wellbeing of these areas.

Retail provisions in Petone Commercial Activity Area – Area 2 are intended to provide a complementary retail role to existing retail activities within the Jackson Street Historic Retail Precinct (Petone Commercial Activity Area – Area 1). Retail activities within the Petone Mixed Use Area are anticipated to be in the form of large format retail and other types of retailing (such as sale of bulky goods) which do not directly

compete with the range of retail goods and services available within the Jackson Street Historic Retail Precinct.

It is anticipated that small-scale retail activity will remain concentrated in the Jackson Street Historic Retail Precinct to protect the economic vitality of this area and its historical value and character. It is important to protect the economic vitality and viability of this area, in order to support the ongoing use of buildings in this precinct to fund the repair, maintenance and other improvements to buildings within this identified Historic Area.

However, it is recognised there may be demand for individual small-scale retail activities in the Petone Mixed Use Area. Applications for small-scale retail activity in the Petone Mixed Use Area are expected to be accompanied by an assessment of their potential effects on the retail efficiency and economic vitality and viability of the Jackson Street Historic Retail Precinct, as well as identifying whether a more suitable location exists for this type of retail within the Jackson Street Historic Retail Precinct. Although it is recognised that individual small-scale retail activities may have minor effects on the economic vitality and viability of the Jackson Street Historic Retail Precinct, the cumulative effect of such development is likely to be detrimental to the economic vitality and viability of this retail area.

The Petone Mixed Use Area is not considered suitable for large-scale integrated retail developments (i.e. above 10,000m<sup>2</sup>) due to potential effects on the economic vitality and viability of the Jackson Street Historic Retail Precinct and Hutt City's Central Commercial Activity Area, as well as potential increases in traffic volumes on the surrounding road network and natural hazard risks. This activity is considered to be more suitably located within Central Commercial Activity Area (commonly referred to as Lower Hutt CBD).

It is anticipated the Petone Mixed Use Area will experience some residential development. Residential activities can positively contribute to the vibrancy and vitality of the area, as these residents have convenient access to retail, commercial, community and recreational amenities within the Petone area. In addition, with changing demographics of an aging population and smaller household units, providing for residential activities within the Petone Mixed Use Area gives another option for meeting the requirements of future residents in the City.

However, residential activities may be incompatible with some other activities in the Petone Mixed Use Area, in particular, they may be sensitive to noise from other activities. Rather than overly restricting other activities, it is appropriate that the residential activities mitigate this sensitivity by providing for external noise insulation. In addition, residential activities are restricted on the ground floor on Jackson Street to ensure activities on the ground floor have a positive relationship to the street where they provide interest, visual connection and an active edge.

Some types of activities have a heavy dependence on private motor vehicles for access, such as supermarkets, takeaway outlets and service stations. Managing these types of activities ensures the effects on the transport network can be effectively assessed.

The traffic effects of larger or more intensive retail, commercial, residential and other non-residential activities is expected to be assessed through the use of a Traffic Impact Assessment. As a guide, a Traffic Impact Assessment is anticipated to accompany most resource consent applications which are expected to generate more than either 50 vehicle movements per hour or 200 vehicle movements per day.

The effects of activities in the Petone Mixed Use Area may have an adverse impact on the character or amenity values of an area or degrade the quality of the environment. These effects need to be managed to maintain and enhance the amenity and other values of the mixed use area and adjacent areas in other zones.

A number of methods are available to avoid or mitigate adverse effects causing harm or damage to the environment. These methods include the use of good urban design, landscaping, use of porous surfaces, sediment traps and other low impact urban design solutions. The Petone Mixed Use Area Design Guide provides advice on the above.

Particular care needs to be taken to avoid adverse effects on biodiversity values, natural character and open space/recreational values of the Korokoro Stream and Petone foreshore, situated outside of the Petone Mixed Use Area.

Localised upgrades to reticulated infrastructure may be required where the capacity is not sufficient to meet the demand for new development. The provision of new or upgraded reticulated infrastructure would need to be designed and constructed to meet the future demand as a mixed use area.

## 5B 1.1.2B Area 2 – Petone Mixed Use - Natural Hazards

### Issue

Petone West is susceptible to a number of natural hazards due to its location in proximity to the Wellington fault line, its coastal position, high watertable, soil profile and anticipated exposure to ground shaking. Development in this area would increase the potential consequences and impacts of these hazards. Given the natural hazard risks and potential consequences, it is necessary to reduce the risk and vulnerability of people and their property from natural hazards.

Risks from natural hazards need to be balanced with risk of negative economic and social consequences from preventing or restricting any further development (and corresponding investment) in this existing urban area and the benefits of allowing for the use of previously developed land in proximity to strategic transport links and a range of amenities.

### Objective

To avoid or mitigate the vulnerability and risk of people and development to natural hazards to an acceptable level.

### Policies

- (a)** Restrict the establishment of certain activities such as emergency facilities, childcare centres, places of assembly, commercial activities accommodating large number of people, education and training, housing for the elderly, and residential facilities in recognition of their higher vulnerability to natural hazard risks and potential for higher consequences in the event of a natural disaster occurring.
- (b)** Manage developments containing higher numbers of people, more vulnerable types of development and developments with higher consequences in the event of a natural disaster, to ensure that the use and development avoids or mitigates the risks posed by seismic hazards to a degree considered acceptable by the Council.
- (c)** Ensure that new development does not occur without an understanding of natural hazard risks.
- (d)** New development does not take place without appropriate precautions and mitigation measures against natural hazard risks.
- (e)** Ensure developments, including buildings and structures, are located, designed and constructed to reduce the risk to building failure and loss of life

from seismic hazards, including fault rupture hazard, subsidence, liquefaction and tsunami inundation.

- (f) Ensure the design and layout of developments, including buildings, avoids or mitigates the effects of sea level rises, taking into account rises to such levels as a result of climate change.

## Explanation and Reasons

The Wellington Fault traverses the Petone Mixed Use Area, which places the area at high risk from seismic activity. This area is at risk from a range of hazards including:

1. Surface fault rupture within the Wellington Fault Special Study Area;
2. Wellington fault subsidence and ground level change;
3. Ground shaking;
4. Liquefaction and associated subsidence;
5. Tsunami inundation; and
6. Climate change impacts, including absolute and relative sea level rise.

Fault rupture has the potential to cause significant damage to buildings, structures and life without warning. The Wellington Fault Special Study Area has been identified to manage the risks posed by fault rupture, although other earthquake-related risks apply outside this area.

A significant portion of the mixed use area has been identified as having a medium to high overall earthquake risk, with liquefaction potential identified as high and very high.

The assessment of hazard risks involves a consideration of potential consequences from natural hazards, in addition to the likelihood of an event occurring.

Due to the potential for large numbers of people to congregate or work in community facilities and large commercial buildings, as well as facilities which provide critical services during emergency events, the establishment of specific activities is restricted. These restricted activities are based on the Building Importance Categories in the Building Code. In addition, any new development will need to undertake a site specific investigation of the seismic hazards (fault rupture, liquefaction, tsunami and subsidence) to determine the risks to people and property posed by the hazards and the measures proposed to avoid or mitigate these risks.

Although the risk of damage from natural hazards can be reduced by the adoption of a range of mitigation measures, natural hazard risks cannot be completely eliminated. Consideration should be given to the risks posed by natural hazards over the anticipated lifetime of development (not less than 50 years).

Prior to any development occurring, it is important there is understanding and consideration of the natural hazard risks and assessment of precautions and mitigation measures proposed in response to these risks. A site specific assessment would ensure these risks are assessed. This assessment needs to take into account the future use and occupancy of the development.

All new buildings and extensions and alterations to existing buildings which increase gross floor area above 5% will require resource consent. Matters of consideration will include natural hazards and will require the submission of geotechnical information provided by a suitably qualified person assessing the seismic risks for the subject site, including fault rupture, ground shaking, subsidence, tsunami and liquefaction. The information shall identify the location and depth of any fault trace

and/or fault trace deformation, location and depth of subsidence, liquefaction risk and risk of tsunami inundation.

At the time of proposing new development, geotechnical information will be required to demonstrate new buildings avoid and setback from being sited directly over a fault trace. For subsidence, liquefaction and ground shaking, geotechnical investigations will be required to assess the ground conditions of the site, and assess the intensity and nature of future development of the site, including building design and construction techniques.

Due to the position of the aquifer under part of the mixed use area, applicant's should be aware that intrusive ground investigation intended as part of an assessment of hazard risk, may also require resource consent from Greater Wellington Regional Council.

The design and layout of development, including buildings, is also expected to take into account methods to avoid or mitigate the risks from sea level rises, and take into account changes to these levels as a result of climate change. Sea levels in Wellington Harbour are predicted to rise by 0.8m by the 2090's or approximately 1m in the next 100 years. This prediction does not take into account relative sea level rises, caused by polar ice-sheet loss and tectonic subsidence.

### 5B 1.1.3 Area 2 – Petone Mixed Use – Main Entrance and Gateway

Amendment 5

#### Routes

##### Issue

**The main entrance and gateway routes of the City need to be attractive and clearly defined to reinforce the identity of the City and the sense of place that it offers. Three of the main transport routes pass through the Petone Mixed Use Area. These routes include Hutt Road, The Esplanade and Jackson Street, which contribute to the character, quality and amenity of Area 2 as a mixed use environment. It is necessary to manage the effects of development along these routes to maintain and enhance the image of the City, whilst enabling utilisation of the sites adjoining these routes consistent with the mixed use character of the area.**

##### Objective

To recognise and enhance the image and visual appearance of the main entrances and gateways of the City where they pass through Area 2 – Petone Mixed Use.

##### Policies

- (a)** Ensure that the design, external appearance, and orientation of structures and buildings maintain and enhance the image and visual appearance of the main entrances and gateways to the City.
- (b)** Ensure that buildings are designed in a manner that maintains and enhances the safety, convenience, accessibility, amenity and linkages within Area 2 - Petone Mixed Use.
- (c)** Manage prominent sites to maximise sense of identity, act as gateways, provide visual interest and provide a visual point of reference in the landscape/streetscape. To be achieved by managing the design and appearance of new buildings and developments, including additions and alterations.
- (d)** Ensure the use of quality urban design directed at enhancing the relationship of buildings with public open space within streets having regard to the role, character and values of each main entrance and gateway route.

## Explanation and Reasons

The objective is to recognise and enhance the image and visual appearance of main entrance and gateway routes within the City. These routes can enhance the experience of entrance to and exit from the City, thereby creating a sense of approach, arrival and departure as people travel through the City. This helps to reinforce the identity of the City and accentuates the sense of movement from one place to another when entering or leaving the City. This approach to the main entrance and gateway routes can assist peoples' sense of orientation and understanding of the physical structure, and the geography, of the City.

Therefore, new buildings and developments and larger additions to existing buildings within the Petone Mixed Use Area will be specifically managed to ensure they achieve this objective. The main entrance and gateway routes within Area 2 – Petone Mixed Use are Hutt Road, The Esplanade and Jackson Street identified in Appendix Petone Commercial 3. Each road has different role, character and values which are to be considered.

In addition, taller buildings on prominent sites will be specifically managed due to their greater visual exposure and their role in creating landmark features. Particular sites in the Petone Mixed Use Area have been identified as prominent sites, with supporting design guidance provided to manage the building design.

### *Hutt Road:*

The eastern side of Hutt Road south of Campbell Terrace is within Area 2 - Petone Mixed Use. The role of Lower Hutt as a regional industrial and service centre is emphasised by the character of the buildings and type of activities along this route. There are significant features along this route that mark progress, including the intersection with Jackson Street, and connection to the Petone Railway Station. This route provides a link between the Petone Off Ramp and the Hutt City Central Commercial Activity Area. Development and activities along this route should enhance the amenity value and visual appearance of this main entrance route.

### *The Esplanade:*

This area forms one of the most important entrances to the City. Given the close association with the harbour and public open space along the Petone foreshore, it is important to maintain a high standard of design for buildings and structures fronting The Esplanade.

The Esplanade carries a significant volume of traffic along the Petone foreshore area. This route is at the edge of the City and is part of the connection with the harbour, which is one of the region's most significant natural features. There is a contrast between, on the one side of the road, a building dominated townscape, and on the other, an open and panoramic seascape.

The building design and appearance will be managed to enhance the amenity value and visual appearance of the area and adjoining foreshore, as well as creating an attractive gateway entrance to the City. Design features will need to be incorporated to prevent visual monotony and promote a strong visual connection with the street. This should include the provision of landscaping along the road front boundary and setting buildings back from the street edge, except on small and narrow sites identified in Appendix Petone Commercial 7.

### *Jackson Street:*

That portion of Jackson Street between Hutt Road and Victoria Street forms an important and highly utilised gateway route into Petone, for both vehicles and pedestrians, with Jackson Street being the main pedestrian thoroughfare extending from the Railway Station to Cuba Street. Given the close association with the historical commercial character of Jackson Street east (Area 1), and surrounding residential

activities, it is important to maintain a high standard of design for buildings and structures fronting Jackson Street.

Activities along Jackson Street will be required to maintain and enhance the amenity value and visual appearance of this route. This will be achieved using specific standards and guidelines for buildings and structures to provide a safe, functional and attractive environment that accommodates a range of activities.

## 5B 1.2 Site Development Issues

### 5B 1.2.1 Area 1 - Distinctive Character and Built Form of the Area on Jackson Street generally between Victoria and Cuba Streets

#### Issue

**Buildings and structures on both sides of Jackson Street generally bounded by Victoria and Cuba Streets have a distinctive built form, style and character. It is important that these characteristics are retained and enhanced.**

#### Objective

To ensure that the distinctive built form, style and character of buildings and structures in the area between Victoria and Cuba Streets are retained and enhanced.

#### Policy

- (a) External alterations, repairs, or modifications to existing buildings and structures plus the construction of new buildings and structures in the area bounded by Victoria and Cuba Streets must comply with the specified design performance standards.

#### Explanation and Reasons

The area on both sides of Jackson Street bounded generally by Victoria and Cuba Streets consists of a mix of one and two storeyed buildings, with small frontage retail activities and commercial activities at road level, and residential flats or offices above. Many of the buildings in this area were built between 1926 and 1940. A large number of buildings have decorative parapets and present an imposing impression from the road.

This area has a distinctive built form, style and character. It is important that these characteristics are retained and enhanced. Council does not seek to prevent or prohibit the repair, alteration, modification or redevelopment of existing buildings or structures. Any such changes to the external facade of existing buildings or redevelopment must not compromise the existing built form and character of the area and will be assessed in accordance with design performance standards specified in Appendix Petone Commercial 1.

Signs on buildings not only provide an important commercial function but also add to the character and vitality of the area. The design of their position, size, shape, colour and lettering style must be carefully considered and assimilated into the design of the building as a whole. Therefore, it is important that all signs are compatible and sympathetic with the distinctive character of the area. This being the case all signs, (except those that are temporary for a period of three months) require a resource



consent and will be assessed in accordance with the design performance standards specified in Appendix Petone Commercial 1.

## 5B 1.2.2 Areas 1 and 2 - Weather Protection

Amendment 6

### Issue

**It is important that all buildings on either side of Jackson Street between Hutt Road and Cuba Streets (Areas 1 and 2) have verandahs to provide weather protection.**

### Objective

To ensure that all buildings on either side of Jackson Street between Hutt Road and Cuba Street (Areas 1 and 2) have adequate weather protection, except those existing buildings designed and built without verandahs in the Jackson Street Historic Retail Precinct (Petone Commercial Activity Area – (Area 1).

### Policy

- (a) To ensure that in the Jackson Street Historic Retail Precinct (Petone Commercial Activity Area - Area 1 all buildings except those existing buildings designed and built without verandahs have verandahs to provide weather protection to pedestrians.
- (b) To ensure that in the section of Jackson Street within Petone Commercial Activity Area - Area 2 all new buildings and additions and alterations to existing buildings have verandahs to provide weather protection to pedestrians.

### Explanation and Reasons

For the comfort of pedestrians/shoppers and to encourage pedestrian movements, it is important that all buildings in Areas 1 and 2 have verandahs. For the section of Jackson Street within Petone Commercial Activity Area - Area 2, it is anticipated that over time the area will be re-developed and the provision of verandahs would create a pedestrian friendly environment that will provide greater comfort and encourage circulation. In the Jackson Street Historic Retail Precinct (Petone Commercial Activity Area - Area 1), an exception is provided where existing buildings were originally designed and built without a verandah.

## 5B 1.2.3 Area 2 - Character and Building Form and Quality within Area 2 - Petone Mixed Use

Amendment 7

### Issue

**The Area 2 - Petone Mixed Use area provides for a range of complementary activities to support the needs of residents and workers. In order for the area to attract and support the mixture of activities, any buildings, structures and associated areas need to be functional, attractive and contribute to the quality of the environment. The building and open spaces also need to recognise their context and effects on their surroundings, such as the foreshore, historic heritage areas, main entrance and gateway routes, and residential areas.**

### Objective

To ensure that the form and quality of buildings, structures, open space and development overall within the Petone Mixed Use Area maintain and enhance the

character, amenity values and quality of the environment, whilst recognising and protecting the values and features of adjoining areas.

## Policies

- (a)** Provide for alterations and minor additions to existing buildings, subject to minimum standards, and encourage a high quality urban and built form design for these building modifications.
- (b)** Manage new buildings and developments and larger additions to existing buildings to be well designed and to contribute to the creation of an integrated, safe and attractive mixed use environment with a high standard of streetscape and amenity.
- (c)** For Jackson Street, require buildings to provide and maintain an active, transparent and continual frontage, as well as shelter, to provide a pedestrian focused environment along this main gateway route.
- (d)** Manage the height and location of buildings to respond to their context and locality, with lower building heights for the areas:
  - a. Adjoining and close to Residential Activity Areas to minimise effects on the amenity values, including shading, over dominance and privacy; and
  - b. Along the Jackson Street front road boundary with taller buildings setback from the street, thereby creating a streetscape with lower level buildings and protecting sunlight to public spaces within the street.
- (e)** Manage the height, location and design of buildings and development on The Esplanade to create a landscaped street frontage and protection of sunlight access to the beach to avoid overshadowing.
- (f)** Encourage all new buildings to provide appropriate levels of natural light to occupied spaces within the building.
- (g)** Require a minimum level of amenity for future occupiers of residential buildings through the use of a permitted activity standard regarding outdoor space. In addition to encouraging good quality and amenity by guiding their design to ensure current and future occupants have useable internal space, ongoing access to daylight, and an external aspect.
- (h)** Manage new buildings to be designed to manage the adverse effect on amenity value, including visual, wind and glare.
- (i)** Restrict the height of buildings and structures at the interface with adjoining residential areas to minimise effects on the amenity values, including shading, over dominance and privacy.
- (j)** Ensure that new buildings higher than 12 metres are designed to avoid, remedy or mitigate any wind problems that they create (including cumulative effects with other buildings) and where existing wind conditions are dangerous, ensure new development improves the wind environment as far as reasonably practical.
- (k)** Encourage buildings to be designed and located in a manner that enhances the safety, convenience, accessibility and amenity of pedestrian spaces and linkages within the Petone Mixed Use Area.
- (l)** Manage the effect of development on adjacent areas identified for their historic heritage, cultural, and distinctive character and built form values.
- (m)** Ensure developments, including buildings and structures, are located, designed and constructed to reduce the risk to building failure and loss of life from seismic hazards, including fault rupture hazard, subsidence and liquefaction.

## Explanation and Reasons

The Area 2 - Petone Mixed Use area consists of a range of complementary activities. In order to provide a quality environment that is attractive, functional and contributes to the quality of the environment, buildings and structures need to be well designed and integrated into the area.

It is recognised there are a variety of existing building forms and styles which have various functions and uses, and are of a mixed quality. The District Plan seeks to ensure the design of new buildings and developments positively contribute to the area's environment by adopting best practice urban design outcomes. Through the development and use of design guidance, the Council will guide and assess the appropriateness of the urban design outcomes resulting from development in the area.

Minor alterations and small additions to existing buildings are provided for to facilitate the upgrading, modification or conversion of the existing building stock in the area. For these small modifications to existing buildings, Council will encourage high quality building design to make a positive contribution to the built character and quality of the central area.

For new buildings and developments and larger additions to existing buildings, these will be specifically managed to ensure they relate well to the public environment and support the overall role of the area as accommodating a mixture of activities, and contribute to the quality of the environment. Buildings will be required to consider the relationship to public environment (such as streets and open spaces), creation or maintenance of linkages within the site and with adjoining sites and streets.

For Jackson Street, one important interface is the ground level relationship between buildings and the streetscape. Requiring display windows and buildings to be located on the front boundary of this street maintains and enhances the quality of the streetscape for pedestrians. In addition, requiring shelter for pedestrians along Jackson Street provides protection from adverse climatic conditions and provides a more comfortable environment between the Petone Railway Station and the Jackson Street Historic Retail Precinct.

One of the highly valued areas of Petone is the foreshore and beach. The foreshore and beach is valued for its recreation (active and passive), cultural, natural and historic values. Creating a landscaped frontage along The Esplanade responds to these values and provides an attractive environment. Protecting sunlight access to the beach year round from over-height buildings or buildings located close to The Esplanade frontage would support the ongoing use and enjoyment of the beach environment.

The large street block formed by Te Puni Street, Jackson Street, Victoria Street and The Esplanade, and the larger properties within this street block and other areas, have poor connectivity (few streets and large blocks) of a type for a good quality mixed use environment. In addition, there is no public open space within the Petone Mixed Use Area. The Design Guidelines contain direction on creating new public open space and/or street connection within the Petone Mixed Use Area

Provision has been made for intensive residential development in the Petone Mixed Use Area. It is important buildings to be occupied for residential living purposes are designed to provide suitable amenity for the future occupants (e.g. natural light and sunlight access, and an external aspect). Encouraging provision for natural light to all habitable and high use areas of new buildings will assist in creating an attractive internal environment and help to reduce the on-going energy requirements of new buildings. Design guidance is provided to encourage quality residential buildings to be developed which provide for these qualities as the Petone Mixed Use Area develops further.

A minimum level of residential amenity for future occupiers is ensured through the use of minimum performance standards regarding outdoor space provision and noise

insulation. Design solutions which meet these performance standards are also likely to provide other internal amenity features and benefits, such as natural lighting and ventilation.

The existing wind speeds at ground level within the Petone Mixed Use Area are variable, with some areas experiencing high and dangerous conditions. In addition, in some locations within this area, such as areas of open space and outdoor street activity, calmer wind conditions are desirable to provide a more attractive environment. The wind conditions contribute to the overall amenity in this mixed use environment, with buildings having a direct relationship with the resultant wind conditions. Accordingly, the District Plan manages new buildings and larger additions to existing buildings over 12 metres in height to ensure the wind conditions are not worsened.

The Petone Mixed Use Area shares an extensive interface with adjacent Residential Activity Areas. This interface is a particularly sensitive one as the effects associated with commercial or servicing activities and development have the ability to adversely impact on the use and enjoyment of neighbouring residential areas.

Given the extent of this interface, and the range of activities permitted within the Petone Mixed Use Area, the District Plan seeks to ensure that adequate safeguards are put in place to protect residential amenity. These safeguards include measures to control the effects of new buildings and development and additions to existing buildings, on adjacent residential areas.

Adjacent to the Petone Mixed Use Area are areas with different values. Jackson Street between Victoria Street and Cuba Street (Petone Commercial Activity Area – Area 1) is recognised for its historic heritage values and distinctive character and built form. Development within the Petone Mixed Use Area adjacent to the section of Jackson Street should recognise and respond to the values, character and form of the adjacent area. The Te Puni Urupa is surrounded by the Petone Mixed Use Area and is recognised for its cultural values (Community Iwi Activity Area). Development adjacent to the urupa is to be managed to protect the cultural values of this area through the use of height controls and design requirements.

The Wellington Fault traverses the Petone Mixed Use Area and is subject to high risk of seismic activity causing fault rupture, liquefaction, subsidence, ground shaking and tsunami. Fault rupture has the potential to cause significance damage to buildings, structures and life without warning. The Wellington Fault Special Study Area has been identified to manage the risks posed by fault rupture. At the time of proposing new development, geotechnical information will be required to demonstrate new buildings avoid and setback from being sited directly over a fault trace. For subsidence, liquefaction and ground shaking, geotechnical investigations will be required to assess the ground conditions of the site, and assess the intensity and nature of future development of the site, including building design and construction techniques.

## 5B 1.2.4 Carparking

Amendment 8

### Issue

**Car parking not contained within buildings can have adverse effects on amenity values. It is important that such car parking is are designed and located to avoid or mitigate adverse effects.**

The provision of on-site car parking can have traffic impacts on the adjacent and wider road transport network. A balance needs to be struck between providing a reasonable degree of on-site parking to meet the car parking needs of existing and future resident and workforce populations, with managing potentially increases in traffic volumes on the wider road network. Providing a high level of on-site car parking, can encourage the use of private vehicles and discourage the use of more sustainable forms of transport.

## Objective

To ensure that adverse visual and transport effects arising from car parking are avoided or mitigated.

## Policies

- (a) Manage the design, location and scale of car parking, servicing, manoeuvring and access to maintain and enhance the streetscape and visual amenity values of the Petone Mixed Use Area.
- (b) Manage the design, location and scale of car parking, servicing, manoeuvring and access, to avoid or mitigate adverse effects on the transport network.

## Explanation and Reasons

The provision of suitable on-site carparking, servicing and access is an important part of a number of activities and developments. Carparking needs to be both adequate and well-located, while not compromising other forms of transport or degrading the streetscape or visual amenity values of the area. The supply of carparking can influence the transport modes people use. The carparking provisions in Chapter 14A(iii) are to be applied in conjunction with these considerations.

Performance standards and design guidance for carparking is provided in the Petone Mixed Use Area Design Guide to ensure on-site carparking is provided in a manner which recognises and reflects the streetscape and visual amenity values of the area.

The use of porous car parking surfaces is also encouraged, which provides a number of environmentally friendly benefits, including a reduction in surface water ponding and contaminants entering the drainage system or Wellington Harbour.

Landscaping and screening of car parking can improve the visual amenity values of an area. It is important therefore that areas within the car parking area and areas adjoining roads are suitably landscaped and screened as outlined in the Design Guide.

# 5B 2 Rules

## 5B 2.1 Area 1 - Both sides of Jackson Street generally bounded by Victoria and Cuba Streets

### 5B 2.1.1 Permitted Activities

- (a) In that area of Jackson Street generally bounded by Victoria and Cuba Streets, shown as Area 1, all retail activities with a gross floor area not exceeding 1,000m<sup>2</sup>.
- (b) Commercial activities with a gross floor area not exceeding 1,000m<sup>2</sup>.
- (c) Residential activities above ground floor level.
- (d) Health care services with a gross floor area not exceeding 1,000m<sup>2</sup>.
- (e) Licensed Premises with a gross floor area not exceeding 1,000m<sup>2</sup>.

#### 5B 2.1.1.1 Area 1 Permitted Activities - Conditions

- (a) **Site Coverage:** Up to a maximum of 100%.
- (b) **Maximum Height of Buildings and Structures:** 10.0m.
- (c) **Landscaping and Screening:**
  - (i) All outdoor storage and servicing areas must be screened so that they are not visible from a road or public place.
  - (ii) At least 5% of car parking areas not contained within a building and adjoining roads must be landscaped and screened.
- (d) **Sites abutting residential activity areas:**

Where a site abuts a residential activity area the following conditions shall apply:

  - (i) The maximum height of buildings is 10 metres. All buildings and structures shall comply with the recession plane requirements of the abutting residential activity area.
  - (ii) Side yard - minimum depth of 3 metres where the site abuts a residential activity area.
  - (iii) Rear yard - minimum depth of 8 metres where the site abuts a residential activity area. This may be reduced if there is a service lane to the rear of the site.
  - (iv) Where a site abuts a residential activity area all outdoor storage and servicing areas must be screened by a close-boarded fence or a fence made of solid material with a minimum height of 1.2m and a maximum height of 1.8m.
  - (v) All car parking areas, not contained within buildings, which abut a residential activity area shall be screened by a close-boarded fence or a fence made of solid material with a minimum height of 1.2m and a maximum height of 1.8m.
  - (vi) Where a site abuts a residential activity area, servicing of activities must not occur between the hours of 10.00pm and 7.00am.
- (e) **General Rules:**

Compliance with all matters in the General Rules - see Chapter 14.

## 5B 2.1.2 Restricted Discretionary Activities

- (a) All redevelopment, alterations, repairing or modifications of any building or structure, except the following:
  - (i) Redecoration, repair or alterations which are internal and not visible from the road or from the road frontage; and
  - (ii) Minor repair or alterations or maintenance to the existing facade of a building or structure which does not require any building consent;

which are Permitted Activities.
- (b) All signs, except those that are temporary for a period of three months which are Permitted Activities.

### 5B 2.1.2.1 Matters in which Council has Restricted its Discretion and Standards and Terms

The matters that Council has restricted its discretion are specified in Appendix Petone Commercial 1. These relate to the following matters:

- (i) Building shape;

- (ii) Buildings on corner sites;
- (iii) Building modulation;
- (iv) Wall materials and openings;
- (v) Silhouette, parapets and cornices;
- (vi) Decoration and colour;
- (vii) Verandahs;
- (viii) Under verandahs; and
- (ix) Signs and lighting.

All resource consent applications will be assessed in accordance with the Standards and Terms specified in Appendix Petone Commercial 1.

### 5B 2.1.2.2 Other Matters

All Restricted Discretionary Activities must comply with other relevant Permitted Activity Conditions.

## 5B 2.1.3 Discretionary Activities

- (a) Except where stated in the General Rules, any Permitted Activity which fails to comply with any of the Permitted Activity Conditions.
- (b) Residential activity on the ground floor of buildings.
- (c) Brothels and commercial sexual services on the ground floor of buildings.
- (d) Brothels and commercial sexual services on a site abutting or directly across the road from schools, pre-school facilities, churches and other similar religious establishments or a residential activity area.

### 5B 2.1.3.1 Assessment Matters for Discretionary Activities

- (a) The matters contained in sections 104 and 105, and in Part II of the Act shall apply.
- (b) The degree of compliance or non-compliance with any relevant Permitted Activity Conditions.

## 5B 2.1.4 Non-Complying Activities

- (a) All other activities not listed as a Permitted, Restricted Discretionary or Discretionary Activity.

## 5B 2.1.5 Other Provisions

- (a) Subdivisions - See Chapter 11.
- (b) Financial Contributions - See Chapter 12.
- (c) Utilities - See Chapter 13.
- (d) General Rules - See Chapter 14.

## 5B 2.2

Amendment 9

## Area 2 - Petone Mixed Use - Area Generally Bounded by Hutt Road, Petone Avenue, Campbell Terrace, Victoria Street, Sydney Street and The Esplanade

### 5B 2.2.1

Amendment 10

### Permitted Activities

- (a)** Retail activities with an individual store size not less than 500m<sup>2</sup> and individual or cumulative total floor space of not more than 3,000m<sup>2</sup>.
- (b)** Commercial activities, with the exception of activities anticipated to accommodate in excess of 300 persons at any one time.
- (c)** Warehouses.
- (d)** Garden centres.
- (e)** Residential Activities, with the exception of
  - (i)** Ground floor level on Jackson Street;
  - (ii)** Housing for the Elderly;
  - (iii)** Residential Facilities.
- (f)** Brothels and commercial sexual services, with the exception of
  - (i)** Ground floor level on Jackson Street;
  - (ii)** Sites abutting or directly across the road from schools, childcare facility, churches and other similar religious establishments or a residential activity area; and
  - (iii)** Within a building which is used or partially used for residential purposes.
- (g)** Commercial garages.
- (h)** Service Industry Activities.
- (i)** Cottage Industry Activities.
- (j)** Service, repair or hire of household goods and services.
- (k)** Research for industrial purposes.
- (l)** The alteration, addition and repair of buildings and structures where the gross floor area of the additions is less than 5% of the gross floor area of the existing building.
- (m)** The alteration, addition and repair of buildings and structures which does not change the external building form (floor area and height) of the existing building.
- (n)** The total or partial demolition or removal of buildings and structures.
- (o)** Minor alterations to existing site activities or land condition:
  - (i)** The replacement of fuel storage tanks and ancillary equipment works within service stations in accordance with the National Environmental Standards for Assessing and Managing Contaminants in Soil to Protect Human Health Regulations 2011 or
  - (ii)** Change to layout or configuration of existing car parks or existing drive-through facility.



## 5B 2.2.1.1 Area 2 Permitted Activities - Conditions

Amendment 11

### (a) **Maximum Height and Recession Plane of Buildings and Structures:**

- (i) 20.0m, provided that –
- (ii) 12.0m on road front boundary of Jackson Street, Hutt Road and The Esplanade with a recession plane of 45° sloping inwards up to 20.0m in height;
- (iii) 12.0m for properties abutting a General Residential Area;
- (iv) 14.0m for properties to the east of Victoria Street, except for those sites which abut the General Residential Activity Area;
- (v) 14.0m for properties on the north side of Jackson Street, between Victoria and Gear Streets.

### (b) **Minimum Yard and Setback Requirements:**

- (i) Buildings and structures abutting an urupa shall have a minimum setback of 3m.
- (ii) Buildings and structures abutting a General Residential Activity Area shall have a minimum setback of 3.0m.
- (ii) Buildings and structures shall have a minimum setback of 10m from The Esplanade front road boundary, except for sites identified in Appendix Petone Commercial 7.

Amendment 12

### (c) **Verandahs, Building Frontages and Display Windows on Jackson Street:**

For sites fronting Jackson Street:

- (i) All buildings shall be built to the front boundary of the site and have display windows along the frontage. The ground floor façade surface shall have a minimum of 60% transparent glass display windows.
- (ii) Buildings and structures shall have a maximum ground floor street frontage width for individual occupiers of 30 metres.
- (iii) Any parts of a building fronting Jackson Street shall have a verandah. The verandah shall meet the following requirements:
  - (1) A minimum clear of 2.5 metres directly above the footpath or formed ground surface;
  - (2) No more than 4 metres (measured at the base of the verandah fascia) directly above the footpath or from ground surface;
  - (3) Extend for the full length of the building;
  - (4) Extend outwards from the front of the building to the far side of the kerbing less than 450mm, or 3 metres whichever is ever the lesser: and
  - (5) Provide continuous shelter with any adjoining verandah or pedestrian shelter.

Amendment 13

### (d) **Landscaping and Screening:**

- (i) At least 5% of car parking areas not contained within buildings must be landscaped. Areas within the parking area and areas adjoining or fronting roads must be landscaped.
- (ii) All areas of outdoor storage shall be screened so that they are not visible from a road or public space.

- (iii) Any surface or ground level parking area shall not exceed a maximum width of 18m along the site frontage or 40% of the site frontage whichever is the lesser.
- (iv) On sites with road frontage to The Esplanade, a planting strip shall be provided to comply with the following conditions:
  - (1) A minimum width of 2.5 metres:
  - (2) Planting shall include at least one specimen tree capable of growing to 5 metres in height within 10 years of planting for every 5 metres of site frontage; and
  - (3) The planting strip shall be planted so as to provide separation between on-site pedestrian and vehicle activities and pedestrian/vehicular activities taking place on the street.

Amendment 14

**(e) Sites abutting Residential Activity Areas:**

Where a site abuts a Residential Activity Area, the following conditions shall apply:

- (i) The maximum building height is 12m. All buildings and structures shall comply with the recession plane requirements of the abutting Residential Activity Area;
- (ii) Rear yard - minimum depth of 3 metres;
- (iii) All outdoor storage, carparking, and servicing areas must be screened so they are not visible from abutting sites in a Residential Activity Area;
- (iv) Servicing of activities must not occur between the hours of 10.00pm and 7.00am.
- (v) No mechanical repair and servicing of motor vehicles, trailers or motor fuelled domestic equipment shall be undertaken on the site.

**(f) Sites abutting or directly opposite the Community Iwi Activity Area:**

Where a site abuts or is situated immediately opposite the Community Iwi Activity Area (Te Puni Urupa), the following conditions shall apply:

- (i) The maximum building height is 8m where a site abuts the Community Iwi Activity Area.
- (ii) For all buildings and structures, a recession plane applies of 2.5m + 45° from all site boundaries with the urupa. This recession plane also applies to properties on the opposite side of Te Puni Street, where it will be measured from the road boundary of the urupa.

Amendment 15

**(g) Noise Insulation:**

- (i) Any habitable room in a building used by a noise sensitive activity shall be protected from noise arising from outside the building by ensuring the external sound insulation level achieves the following minimum performance standard:

$$DnT,w + Ctr > 30 \text{ dB}^1$$

Compliance with this performance standard shall be achieved by ensuring habitable rooms are designed and constructed in a manner that:

<sup>1</sup>  $DnT,w + Ctr > 30 \text{ dB}$  is the standardised level difference (outdoor to indoor) and is a measure of the airborne sound insulation provided by the external buildings envelope (including windows, walls, ceilings and floors where appropriate)

- (a) accords with the schedule of typical building construction set out in Appendix Central Commercial 7 – Noise Insulation Construction Schedule; or
- (b) accords with an acoustic design certificate signed by a suitably qualified acoustic engineer stating the design as proposed will achieve compliance with the above performance standard.

(ii) Ventilation

Where bedrooms with openable windows are proposed, a positive supplementary source of fresh air ducted from outside is required at the time of fit-out. For the purposes of this requirement, a bedroom is any room intended to be used for sleeping. The supplementary source of air is to achieve a minimum of 7.5 litres per second per person.

**(h) Outdoor Living Areas for Residential Activities**

- (i) A minimum area of 20m<sup>2</sup> per residential unit shall be provided as either private or shared outdoor amenity space. Of this area, a minimum of 2.5m<sup>2</sup> shall be private outdoor space which is contiguous with the main living area of the unit.

Amendment 16

**(i) Lighting:**

Any activity shall comply with the following requirements:

- (i) The emission of light (including glare) shall ensure that direct or indirect illumination does not exceed 8 lux (lumens per square metre) at the windows of residential activities; and
- (ii) Subject to the above standard, pedestrian routes and carparks available for public use during hours of darkness shall be lit at a minimum of 10 lux, measured in accordance with AS/NZS 1158.3.1 : 2005 and amendments.

Amendment 17

**(j) Dust:**

Any activity shall not create a dust nuisance at or beyond the boundary of the site to the extent it causes an adverse effect. This standard applies to contaminants which are not subject to a discharge consent and which are temporary or intermittent in nature.

**(k) Odour**

All activities shall be carried out in such a manner so as to ensure that there is not an offensive or objectionable odour or fumes beyond the boundary of the site.

Amendment 18

**(l) General Rules:**

Compliance with all matters in the General Rules - see Chapter 14.

## 5B 2.2.2 Restricted Discretionary Activities

Amendment 19  
& 20

Amendment 21

- (a) The construction, alteration of, addition to buildings and structures, except for those works permitted under Rule 5B 2.2.1 (l) and (m).**

## Matters in which Council has Restricted its Discretion

- (i) Design, external appearance and siting of the building or structures
  - (ii) Matters in the Petone Mixed Use Area Design Guide.
    - A Design Statement will be required which demonstrates how the proposed development responds to the design guidelines of the Petone Mixed Use Area Design Guide.
  - (iii) Amenity Values
    - Effects upon the amenity values both within the site concerned and upon surrounding areas from buildings, structures and use of outdoor areas.
  - (iv) Landscaping
    - The extent to which landscaping is incorporated to achieve high quality urban design which maintains or enhances the image and visual appearance of the mixed use area.
    - A landscape plan will be required. This plan should include landscaping of any outdoor on-site parking areas.
  - (v) Traffic Effects, including effects on the transport network from the volume and type of traffic generated, and the suitability of site access and site servicing arrangements.
    - A Transportation Impact Assessment may be required where proposed development is expected to generate more than either 50 vehicle movements/hour or 200 vehicle movements/day.
  - (vi) Natural Hazards
    - The outcomes of the geotechnical investigation on seismic hazards, including fault rupture, subsidence, tsunami and liquefaction.
    - Whether the potential risk to the health and safety of people and property from fault rupture, subsidence, tsunami, liquefaction and sea level rise (taking into account changes to these levels arising from climate change), can be avoided or mitigated.
    - The design and layout of the development, including buildings, to avoid or mitigate the effects from fault rupture, subsidence, tsunami, liquefaction and sea level rise (taking into account changes to these levels arising from climate change).
  - (vii) Capacity of Infrastructure
    - The capacity of the City's infrastructure to service additional development on the site.
  - (viii) Impact on Historic Heritage
    - Expected or potential impacts on the historic heritage values of any adjacent Historic Area, Historic Building or Significant Cultural or Archaeological Resource and any measures to be adopted to protect these values.
- (b) The construction, alteration of, addition to buildings and structures over 12 metres in height, except:**
- (i) The alteration of, addition to buildings and structures where the gross floor area of the additions is less than 5% of the gross floor area of the existing building; or

- (ii) The alteration of, addition of buildings and structures which does not change the external building form (floor area and height) of the existing building.

Matters in which Council has Restricted its Discretion and Standard and Terms

- (i) The effects of wind on public space and adjoining areas.

Amendment 23

- (c) **Any Permitted Activity which fails to comply with any of the relevant Permitted Activity Conditions, or relevant requirements of Chapter 14 - General Rules is to be assessed as a Restricted Discretionary Activity unless:**

- (i) Any non-compliance with rules in Chapter 14 – General Rules, is specifically identified as requiring assessment under an alternative activity status.

Matters in which Council has Restricted its Discretion

- (i) Any actual or potential adverse effects arising from the proposed non-compliance, and measures to avoid, remedy or mitigate such effects.

Amendment 24

5B 2.2.2.2 Restricted Discretionary Activity - Conditions

Amendment 25

- (a) **All Restricted Discretionary Activities shall comply with the relevant Permitted Activity Conditions.**

Amendment 26

- (b) **The construction, alteration of, addition, and repair of buildings and structures over 12 metres in height.**

- (i) Wind Protection:

All buildings and structures over 12 metres in height and where any part of the building or structure fronts a street, pedestrian mall, pedestrian walkway, shall be designed to comply with the following conditions:

- (a) Safety: The safety criteria shall apply to all public space. The maximum gust speed shall not exceed 20 m/s. If the speed exceeds 20 m/s with the proposed development, it must be reduced to 20 m/s or below.
- (b) Cumulative Effect: The cumulative criteria shall apply to all public space. Any proposed development shall comply with the requirements for both of the following wind strengths, at each measurement location.

Wind strength	Change in annual hours of occurrence with the development at all measurement points	Requirements on developer
Strong (mean hourly wind speed = 3.5 m/s)	If hours that 3.5 m/s is equalled or exceeded increase by more than	Reduce change in hours to a maximum of 170 hours.

	170 hr/yr (i.e. 2 % of the year)	
Moderate (mean hourly wind speed = 2.5 m/s)	If hours that 2.5 m/s is equalled or exceeded increase by more than 170 hr/yr (i.e. 2 % of the year)	Reduce change in hours to a maximum of 170 hours.

- (c) While hours exceeded at some locations in the Cumulative Effect Criteria may increase or decrease, the overall impact of a building on the wind conditions must be neutral or beneficial.
- (d) To show that a development complies with these standards a wind report must be supplied that meet the requirements outlined in Appendix Petone Commercial 4 – Wind Report.

## 5B 2.2.3 Discretionary Activities

Amendment 27
Amendment 28

- (a) Except where stated in the General Rules, any Restricted Discretionary Activity which fails to comply with any of the relevant Permitted and Restricted Discretionary Activity Conditions, or relevant requirements of Chapter 14 - General Rules.

Amendment 29
Amendment 30
Amendment 31

- (b) Residential activities at ground floor level on Jackson Street.
- (c) Service stations.
- (d) Retail activities with an individual or cumulative total floor space of more than 3,000m<sup>2</sup>.
- (e) Retail activities with a gross floor area below 500m<sup>2</sup> per store.
- (f) Licenses premises.
- (g) Childcare facility.
- (h) Places of assembly.
- (i) Community activities/facilities.
- (j) Education and training facilities.
- (k) Commercial activities anticipated to accommodate more than 300 persons at any one time.
- (l) Housing for the Elderly.
- (m) Residential Facility.

Amendment 32
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- (n) Car sales yards.

Amendment 33
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- (o) Industrial Activities except for:
- (i) service, repair or hire of household goods and services;
- (ii) research for industrial purposes.

Amendment 34

- (p) All other activities not listed as a Permitted, Restricted Discretionary or Non-Complying Activity.
- (q) Brothels and commercial sexual services on the ground floor level on Jackson Street.
- (r) Brothels and commercial sexual services on a site abutting or directly across the road from schools, childcare facility, churches and other similar religious establishments or a residential activity area.

### 5B 2.2.3.1 Assessment Matters for Discretionary Activities

Amendment 35

- (a) The matters contained in sections 104 and 105, and in Part II of the Act shall apply.
- (b) Design, external appearance and siting of the building or structures
- (c) Matters in the Petone Mixed Use Area Design Guide.
  - A Design Statement will be required which demonstrates how the proposed development responds to the design guidelines of the Petone Mixed Use Area Design Guide.
- (d) Natural hazards, including geotechnical investigation on seismic hazards, including fault rupture, subsidence, tsunami and liquefaction, and measures to avoid or mitigate the effects from fault rupture, subsidence, tsunami, liquefaction and sea level rise (taking into account changes to these levels arising from climate change).
- (e) Amenity Values
  - Effects upon the amenity values both within the site concerned and upon surrounding areas from buildings, structures and use of outdoor areas.
- (f) Landscaping
  - The extent to which landscaping is incorporated to achieve high quality urban design which maintains or enhances the image and visual appearance of the mixed use area.
  - A landscape plan will be required. This plan should include landscaping of any outdoor on-site parking areas.
- (g) Traffic Effects, including effects on the transport network from the volume and type of traffic generated, and the suitability of site access and site servicing arrangements.
  - A Transportation Impact Assessment may be required where proposed development is expected to generate more than either 50 vehicle movements/hour or 200 vehicle movements/day.
- (h) Capacity of Infrastructure
  - The capacity of the City's infrastructure to service additional development on the site.
- (i) Impact on Historic Heritage
  - Impacts on the historic heritage values of any adjacent Historic Area, Historic Building or Significant Cultural or Archaeological Resource and any measures to be adopted to protect these values.

Amendment 36

- (j) The degree of compliance or non-compliance with any relevant Permitted or Restricted Discretionary Activity Conditions.

#### **5B2.2.3.1 (c) Additional Assessment Matters for Small Scale Retail below 500m<sup>2</sup>**

Economic effect on the Jackson Street Historic Retail Precinct including:

- Incremental effect of proposed store(s) on the economic vitality and viability of the Jackson Street Historic Retail Precinct;
- Cumulative effect of proposed store(s) of development proposed and any other small-scale retail approved in the Petone Mixed Use Area over the past five calendar years;
- Effect on the economic efficiency of the Jackson Street Historic Retail Precinct as a retail centre;
- Number of existing vacancies in the Jackson Street Historic Retail Precinct at the time of lodging resource consent; and
- Whether a suitable location is available for the development within the Jackson Street Historic Retail Precinct.

#### **5B2.2.3.1 (d) Additional Assessment Matters for Licensed Premises**

Economic effect on the Jackson Street Historic Retail Precinct including:

- Incremental effect of proposed store(s) on the economic vitality and viability of the Jackson Street Historic Retail Precinct;
- Cumulative effect of proposed store(s) of development proposed and any other small-scale retail approved in the Petone Mixed Use Area over the past five calendar years;
- Effect on the economic efficiency of the Jackson Street Historic Retail Precinct as a retail centre;
- Number of existing vacancies in the Jackson Street Historic Retail Precinct at the time of lodging resource consent; and
- Whether a suitable location is available for the development within the Jackson Street Historic Retail Precinct.

#### **5B2.2.3.1 (e) Additional Assessment Matters for Retail above 3,000m<sup>2</sup>**

Economic effect on the Jackson Street Historic Retail Precinct including:

- Economic effect of proposed store(s) on the economic vitality and viability of the Jackson Street Historic Retail Precinct over a minimum time period of 5 years;
- Economic effect of proposed store(s) on the economic vitality and viability of the Hutt City Central Commercial Activity Area (Lower Hutt CBD) over a minimum time period of 5 years; and
- Identification of the proportion of floorspace to be used for 'comparison'<sup>2</sup> and convenience goods .

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<sup>2</sup> Comparison goods refers to the range of goods which are typically provided for in existing centres, such as books, clothing, music, footwear, cosmetics, accessories, stationary, health and beauty products



## Rule 5B 2.2.4 Non-Complying Activities

Amendment 37

- (a) Emergency facilities.
- (b) Integrated Retail Developments with a gross floor area in excess of 10,000m<sup>2</sup>.

### 5B 2.2.4 Other Provisions

- (a) Subdivisions - See Chapter 11.
- (b) Financial Contributions - See Chapter 12.
- (c) Utilities - See Chapter 13.
- (d) General Rules - See Chapter 14.

## 5B 3 Anticipated Environmental Results

Amendment 38

- (a) The distinctive built form, style and character of buildings in Petone Commercial Activity Area 1 are retained and enhanced.
- (b) Amenities of adjoining properties in Residential Activity Areas will be protected.
- (c) The commercial and retail needs of residents and other users will be met.
- (d) The Jackson Street Historic Retail Precinct continues to be vital and vibrant, with the mixed use area catering for increased diversity of complementary activities.
- (e) A safe and attractive Petone Commercial Activity Areas 1 and 2.
- (f) A sense of place and identity that reflects the character, qualities and context of the Petone area.
- (g) Protection of the biodiversity and recreational values of Korokoro Stream and Petone foreshore situated outside the Petone Mixed Use Area.
- (h) Protection of the historic character and economic vitality and viability of the Jackson Street Historic Retail Precinct.
- (i) A reduction in hazard risks for people and property within Petone Commercial Activity Area 2.

## Appendix Petone Commercial 1

Existing Design Guidance for Petone Commercial  
Activity Area 1 – Jackson Street Historic Retail Precinct

# Appendix Petone Commercial 1

## Part 1: Building Shape

### 1.1 Background

Given the quality of old building stock fronting Jackson Street, refurbishments or new developments should reinforce the visual cohesion of the existing facades. Refurbishment or renovation of existing buildings should relate to the historical design traditions within the street.

### 1.2 Design Performance Standards

The design performance standards for the assessment of building shape are:

1. The extent to which building refurbishment or new development is designed with consideration for both;
  - (a) The historical design characteristics with Jackson Street.
  - (b) Those buildings adjacent to the proposed refurbishment or redevelopment.

See Figure 1.

2. The extent to which building refurbishment, renovation or replacement is designed to maintain the compatibility of cornice lines, floor to floor heights where these are strongly expressed, sign bands and other elements in adjacent buildings and strives to unify the street as a whole.
3. That buildings be built to maintain the compatibility of the streetscape frontage.

*Explanation: Buildings in Jackson Street are generally built up to the front boundary and this is a common unifying element in the streetscape. However, it is appropriate to consider situations where a building and the space created between the building and the street may together contribute to an interesting streetscape as a result of contrast.*

4. The extent to which the new building is compatible with adjacent building heights.

*Explanation: Buildings in the area are generally single or two storey in nature. To encourage the strengthening of the traditional linear street form new developments will comply with this height requirement.*

See Figure 2

5. Where single storey buildings are proposed and adjoining buildings are higher the extent to which designs use high parapets, false fronts and cornices to approximate more closely the average height of the neighbouring facades will be important.

See Figure 3

## Part 2: Buildings on Corner Sites

### 2.1 Background

Opportunities exist at each street corner to emphasize the character of the building (and therefore the streetscape) and to make the building form three dimensional by creating an L-shaped facade.

Due to the prominence of corner sites, buildings in these locations have the potential to become landmarks. Emphasis can be achieved by chamfering the corner and introducing special elements such as towers, turrets, clocks and elaborate decoration, and corner entrances. Emphasis is also achieved by encouraging stronger vertical elements, such as doorways with a pediment, or full height columns. Where all the corner buildings at one intersection have used their position to advantage, the street pattern benefits from the drama created. Corner buildings also act as "book ends" for the buildings in between.

See Figure 4.

## 2.2 Design Performance Standards

The design performance standards for the assessment of buildings on corners are:

1. The extent to which refurbishment or redesign of corner buildings emphasizes their corner location.
2. The extent to which building renovation or redevelopment includes the use of vertical elaboration in parapet and/or corner tower architectural features. Corner entrances and canopies with strong facade modulation will evoke a particular focus, acknowledge and celebrate the corner with all levels of the building.

## Part 3: Building Modulation

### 3.1 Background

The modulation of a building is the way the design divides up the facade into horizontal and vertical elements, resulting in a three dimensional pattern.

In Jackson Street the pattern is often symmetrical and provides a rhythm along the street with horizontal elements overpowering the vertical. Strong horizontal bands define the levels in the building. These are particularly the line of the verandah, cornice line and the parapet silhouette.

See Figures 5 & 6.

### 3.2 Design Performance Standards

The design performance standards for assessing building modulation are:

1. The extent to which building designs create a total building shape which reflect the traditional horizontal and vertical proportions and symmetry of building in Jackson Street.
2. The extent to which building designs emphasize the traditional strong horizontal elements of the verandah, cornice line and the parapet silhouette.

3. The extent to which the vertical lines will be less accentuated in the building design and occur as the structural bay columns of the building and the details of individual components such as doorways and shop front details.
4. The extent to which buildings which are continuous across a number of street level shops include modulation which is consistent at first floor level but is broken into rhythmical bays at parapet level to reflect the shop division below.

See Figure 7

## Part 4: Wall Materials and Openings

### 4.1 Background

The buildings in Jackson Street are predominately (80%) plastered brick work or reinforced concrete, while 20% are of weatherboards over timber. Sometimes one is made to look like another. A monolithic form dominates, i.e. the building appears to be solid with openings shown as punctuation rather than transparent.

Often the thickness of the wall is emphasised by the built up reveal around openings, creating a shadow pattern. This can occur at parapet level where the thickness of the wall can be seen on its edges and cut outs.

There is a hierarchy in the size of the windows of a building, progressing from large at street level, and reducing in size and scale in the levels of the facade.

Along the street level, shop front glass covers most of the wall area, responding to the retailers need for display space. However structural columns are expressed at each bay and windows generally start a minimum 600mm above the footpath the shop fronts are divided by glazing bars to increase interest and reduce the scale. Often decorative tiles are used below sill level and the upper panes of glass are lead lights.

See Figure 8

At first floor level, windows occur rhythmically along the facade, either as single vertical units at frequent intervals, or in groups of windows, where the proportion is square or rectangular. Some buildings have reinforced the shape of groups of windows by making them into bay windows.

### 4.2 Design Performance Standards

The design performance standards for the assessment of wall materials and openings are:

1. The extent to which the building design reflects the traditional pattern of wall materials and openings.
2. The extent to which the building will appear monolithic rather than having a skin or veneer.
3. The extent to which building designs have discrete openings, and decoration which provides a rhythmical pattern within the monolithic form.
4. The extent to which building designs follow the general pattern of display windows at ground floor and rhythmic units on upper floor. If windows are grouped their segments will be highlighted by solid glazing bars. The extent to

which openings may be embellished with decorative surrounds which together with the variation in groups, will add interest to the building facade.

5. Large bands of glass uninterrupted by areas of wall, or patterns of glazing bars ARE NOT acceptable as they do not respond to the street's history or character.

## Part 5: Silhouette, Parapets and Cornices

### 5.1 Background

Due to the varied heights of buildings and their definite parapet patterns along the street the silhouette is varied and interesting. It provides a vitality to the streetscape above the verandah which is very visible to the pedestrian at street level. Many of the buildings have lost much of the embellishment of earlier times. Accurate reconstruction of missing external details is encouraged on heritage buildings.

The parapet creates an illusion of height. This, together with decorative features including an intricate parapet outline and embellishments, and below, ornate cornice lines and or applied signs and decoration, create imposing facades to the buildings.

Almost all the buildings have a strong cornice line applied to the face of the building. This strong horizontal line, emphasised by the shadow it creates underneath, is a dominant feature.

See Figure 9.

### 5.2 Design Performance Standards

The design performance standards for the assessment of silhouette, parapets and cornices are:

1. The extent to which building design includes a parapet, the size and proportions of which shall relate to the rest of the building, both in height and complexity and the design of adjacent buildings.
2. The extent to which the buildings design includes decorative skyline features, these might include urns, balls, balustrades etc. constructed in modern materials which are not heavy masonry and therefore able to be fixed without compromising structural stability. Other appropriate parapet features include pediments, towers or cupolas, flag poles and turrets.
3. That buildings design will include a cornice line.

## Part 6: Decoration and Colour

### 6.1 Background

Both decoration and colour, whilst being an integral part of a buildings character, can be replaced, added to or altered. These elements provide an opportunity to emphasise the character of the street itself.

Decoration should generally be applied as a complex pattern of small scale elements, which add up to an overall pattern and give an identity to the building. Decoration also indicates scale, adds stability and visual delight, and creates shadow effects.

See Figure 10.

Many older buildings in Jackson street have had their decorative features removed, partly in response to the perceived earthquake danger, and partly as architectural style changes to a more “modern” and uncluttered style. It is now recognised that the decorative features are important both to the character of individual buildings, and to the vitality of the street. Accurate reconstruction of missing external details is encouraged on heritage buildings.

## 6.2 Design Performance Standards

The design performance standards for decoration and colour are:

1. The extent to which the building design modulates its street facades with structural and decorative elements which recognise and respond to the diversity of the street in general and their neighbours in particular.
2. The extent to which renovations and alterations to older buildings reintroduce decorative features.
3. The extent to which colour schemes for buildings are designed to emphasise the decorative and structural elements of the facade.
4. The extent to which the overall colour scheme relates to both above and below verandah level.

## Part 7: Verandahs

### 7.1 Background

The function of the verandah is protection from wind rain and summer sun. It was originally designed as an integral part of the building and was used to achieve a visual transition from facade to street.

Verandahs occur on almost all buildings, with corner buildings being the main exceptions. There are two traditional shapes. These are flat verandahs held up with hangers, or sloping verandahs with posts

Many buildings with flat verandahs have windows just above the verandah. This allows natural light into the high stud shops at street level.

Although the verandah was built primarily as a transitional shelter space, it also forms an enclosed space of human scale at street level. The arcade like atmosphere created by verandah posts is visually attractive to the pedestrian and provide separation from traffic. The use of verandah posts in Jackson Street is encouraged.

Verandah fascias are flat horizontal bands used as a facing to the street. Their main use is for applying signs, which indicate the name of the business available.

When verandahs are designed as an integral part of the building the fascia was often deep and modulated, reflecting the forms of the parapet, emphasising a detail or entry.

See Figure 11.

## 7.2 Design Performance Standards

The design performance standards for verandahs are:

1. The extent to which building designs include verandahs based upon traditional designs.
2. The extent to which verandah designs include a modulated hierarchy of facia elements (perhaps responding to a significant point of entry) and vertical modelling of verandah details to emphasize variety of form and reflects features of the building.

## Part 8: Under Verandahs

### 8.1 Background

Shop fronts are the dominant visual element under the verandah, competing with each other to provide the commodities and services we require. Well designed shop fronts can enhance the street and compliment the design of the buildings in which they are set. Many existing shop fronts are subject to pressure for regular refurbishment, to maintain a “progressive” retailing image for the occupants, and so many have a relatively short life span. Because of this, and to maintain an overall street character, guidelines are desirable for existing and future occupants, developers and designers.

Entrances to shops are traditionally either centrally located with display windows each side (larger shop fronts) or recessed on one side of a more dominant display window. The recess allows a space for the shopper to pause and browse. A succession of these recesses, often reflecting the structural bays of the building above, provides a rhythm along the footpath and the street.

See Figure 12.

The position of the doorways within the bays of shops can be emphasised by a reflecting pattern on the verandah fascia, by a corresponding placement of verandah posts.

The use of glazing bars within shop fronts are an important historical detail. They give an intimate feel, consistent with the size of the shop behind, and introduce an opportunity for tile and leadlight decoration.

### 8.2 Design Performance Standards

The design performance standards for under verandahs are:

1. The extent to which new building, renovation or alteration design reflects traditional designs in the street. Entrances to shops should be either centrally located with display windows either side or recessed on one side of a more dominant display window.
2. The extent to which small retail units (or small frontage units to larger retail floor space) are included to re-establish a reference to rhythm of original building modulation.
3. Recess doorways are preferred.
4. The extent to which detailed design features within the shop front will coordinate with the overall horizontal and vertical symmetry of the facade design.



5. During renovation or reconstruction the extent to which structural or decorative references to the facade above the verandah are reintroduced.

## Part 9: Signs and Lighting

### 9.1 Background

Advertising signs can have a dramatic effect on the whole appearance of a building facade, and character of the street as a whole. This effect can be positive or detrimental to the streetscape and quality of the environment.

Signs on facades or fascias are not isolated entities : they are part of the building facade. It is accepted that signs are an essential part of the commercial character of the area, but a balance must be achieved between commercialism and architectural and streetscape quality. In Jackson Street signage is a significant part of the vibrant, colourful street image, but this is not encouraged to the extent that appreciation of the architectural, historical and character qualities are unduly compromised.

The design of their position, size, shape, colour and, for signs, lettering style, must be carefully considered and assimilated into the design of the building as a whole. The result will either detract from or add to both the character and vitality of the street, the building, and the activity carried out within the building.

See Figure 13.

As a general rule, signs were originally painted or formed in plaster work on flat panels of the building facade : on the pediment or parapet, under the cornice line, on a frieze panel between floors, on glass panes in windows, or on the verandah fascia.

The layout of signs were always symmetrical about a central axis, and signs were rectangular or followed the shape of the architectural surfaces to which they were applied (e.g. a pediment). Lines were horizontal or curved, with an accompanying decorative pattern.

Letters were generally dark on a light background, and gold leaf was used on glass. The most common lettering styles, especially in older buildings, were Antique, Fat Clarendon and Sans Serif.

Less common were Tuscan, Fat-faces, Fat Italics, Fat Gothic and Sans Serif Compressed.

See Figure 14.

Usually only one type face was used, except where one described the owner, and another the type of business.

A well lit shop front or building and attractive window displays tend to attract customers and increase trade. In addition well lit shops and under verandah areas are a method of providing security both to premises and passing pedestrians. Strip fluorescent should be avoided in favour of spot lighting or lighting which emphasise the architectural patterns, e.g. structural bays, verandahs posts, lead lighting.

### 9.2 Design Performance Standards

The design performance standards for signs and lighting are:

1. The extent to which signs related to and assimilated into the design of the building as a whole.

2. The extent to which signs refer to the owner or name of the shop or business rather than to any product which is being retailed.
3. Signs will not be hung at an angle to the building, unless below the verandah.
4. The extent to which lighting is to be used to dramatise the shape and decoration of the building as well as to highlight signs.
5. Lighting and illuminated signage above the verandah levels must be sensitive to residential uses. Spot lighting or general floodlighting down the facade, which reinforces the architectural character of the building, or which highlights particular features or signs, are encouraged.
6. The extent to which below the verandah level lighting is provided within any design.

# Appendix Petone Commercial 2 – Petone Mixed Use Area – Design Guide

Amendment 39

Amendment 40



# Petone Mixed Use Area Design Guide





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## 3. Appendix

## 1.1 Objectives of the Design Guide

The Petone Mixed Use Area Design Guide provides the basis for the design assessment of new development in this area. The purpose of the guide is to (a) assist the implementation of the District Plan's policies for the area, (b) assist the achievement of the Hutt City Council's (HCC) strategic objectives for the area as set out in the Petone Vision Statement; and (c) assist the area to transition from its current physical condition to a mixed use environment that reflects good urban planning and design practice.

The Design Guide is to be used by:

- HCC to evaluate development proposals as part of the resource consent process; and
- Property owners, developers, builders, designers and planners who are considering investment, designing developments, and preparing consent applications.

Design guide's are a tool commonly used throughout New Zealand in circumstances where the Councils are seeking to improve urban environmental quality. In this regard, the Design Guide for the Petone Mixed Use Area is similar to that for the Hutt City Central Area Design Guide in terms of its aims and, in some respects, content.

It is noted that the Design Guide applies principles of good urban design. The NZ Urban Design Protocol, to which Hutt City Council is a signatory, is a reference.

The implementation of the Design Guide will be undertaken by the HCC. However, it's success will rely on landowners, developers and their consultants sharing the common vision for Petone's future and working with HCC through the Design Guide to help achieve it together.

## 1.2 How the Design Guide Relates to the District Plan

Under the District Plan rules, all new buildings within the Petone Mixed Use Area will require a resource consent from HCC. Small scale alterations and additions are exempt from the rules, in recognition that their influence on the quality of the environment will generally be insignificant. Aside from small scale alterations and additions, new building developments are to be assessed against the Design Guide.

The Design Guide is to be applied in conjunction with the other rules and standards in the District Plan. These rules and standards relate to such matters as transportation, historic heritage, notable trees, signage and network utilities.

The Design Guide offers some flexibility to allow innovation and good design solutions that meet the objectives of this document. Inconsistency of development proposals with the Design Guide principles can be a basis for the HCC to decline resource consent application.

Despite this, the Design Guide is just that – a guide. In using the Design Guide to evaluate applications, HCC will adopt a flexible approach rather than an absolute one. It is acknowledged that strict adherence will not always be possible or practical. HCC will balance Design Guide suggestions with broader considerations and practicalities, including commercial viability.



The Design Guide recognises the variation in street types, adjacent uses and objectives for the transition of the area over time to a more mixed use environment, including residential alongside commercial uses.

The illustrations in the Design Guide are indicative only and are intended to further explain the design outcome sought as outlined in the text. They should not be seen as actual design solutions as innovative and creative design solutions that meet the intended future character of the areas are encouraged.

### 1.3 How the Design Guide Relates to Petone Vision

The Design Guide has been prepared taking into consideration the elements contained in the Petone Vision to 2027. Appendix 1 describes the Vision elements and corresponding actions, and references the parts of the Design Guide which can assist to give effect to them.

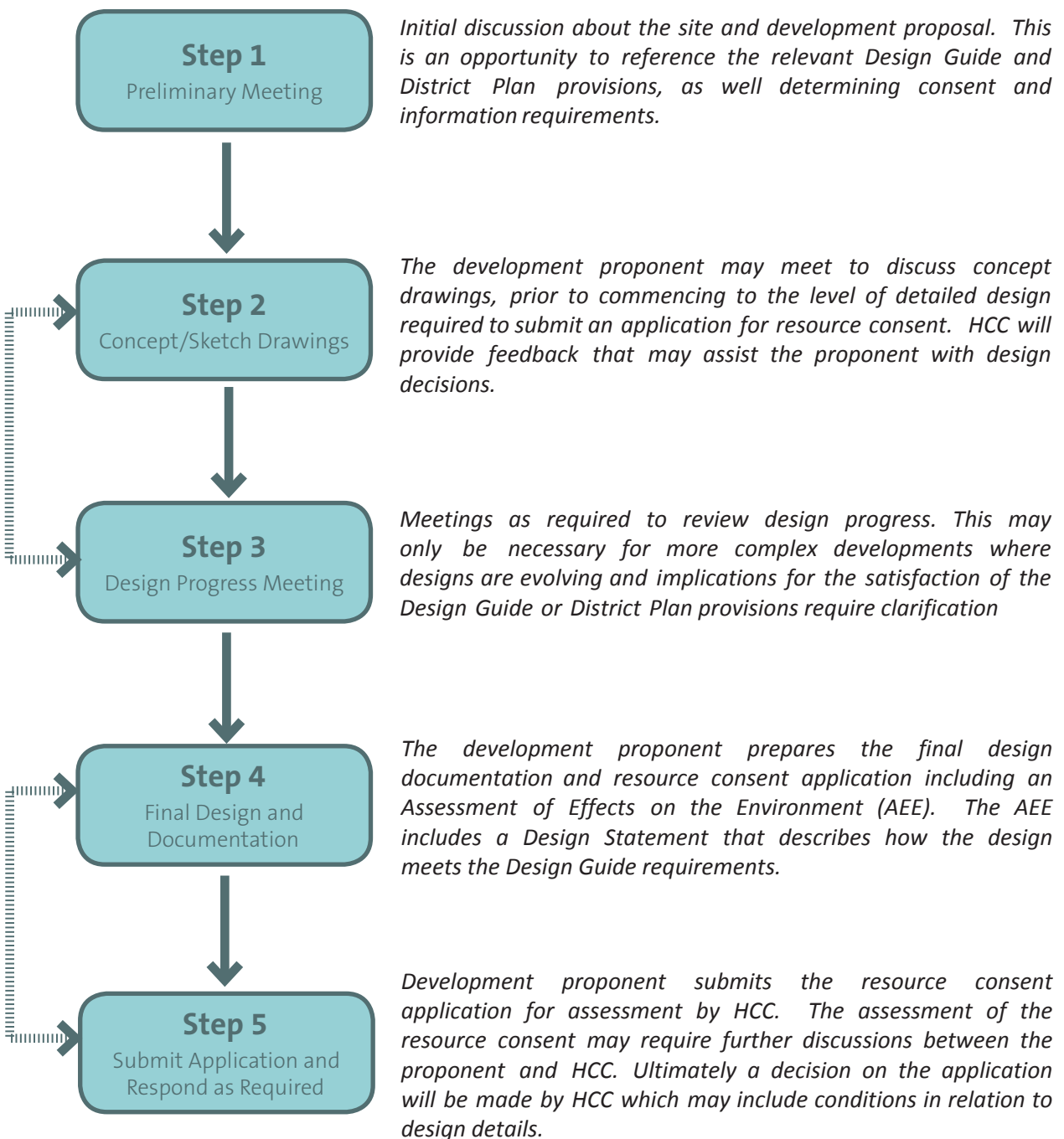
It is important to recognise that:

- The Petone Vision applies to the whole Petone area, not just the area covered by the Design Guide. Accordingly, in applying the vision to Petone West, the variation between the areas is important.
- Some of the existing characteristics of Petone West (refer to section 1.7) influence and, to an extent, limit what is reasonably possible for its future and the Design Guide needs to be cognisant of these characteristics.
- The Petone Vision includes elements which cannot be achieved by the Design Guide and the District Plan. Similarly, the District Plan and Design Guide cannot be expected to single-handedly achieve the Vision as there are many other influences on the future shape of the Petone West area.
- Many of the Vision elements and actions relate to ensuring the qualities of Petone are not adversely affected by change. There are some qualities in Petone West that are positive and these should be retained (e.g. Te Puni Urupa). However, most of its current quality is poor and the provisions of the District Plan and the Design Guide seek to change them.

### 1.4 Approach to Working with Development Proponents

The Hutt City Council encourages landowners, developers, architects, landscape architects, planners and any other parties involved in the development of Petone West to work corroboratively throughout the development planning process.

An important part of the process is early collaboration before concepts are committed to. This enables ideas to be discussed freely prior to commencing detailed design. With initial feedback from HCC, the design and planning time that follows can be expended with some certainty as to HCC’s likely position. A diagram of the desired process is described below. The need for all these steps will depend on the scale of the development proposal. Although this process is optional, it is intended to facilitate an efficient design and consenting process.



## 1.5 How to Use the Design Guide

Each section of the Design Guide is generally structured such that the content includes the elements (title, objectives, explanation, guidelines, images) set out below:

### 2.1 Building Form

Building form refers to the vertical and horizontal scale of a building. Human scale means sizes and dimensions that are not dominating to people.

The objective of managing building form is to generate an environment in which people do not feel dominated by the scale of buildings and so enjoy being there - this leads to more intensity of use, more business opportunities and makes it an attractive choice relative to other centres.

Changes in building volumes (height x width x length) and variation in the external walls can reduce the impression of the scale of large developments. Elements such as verandahs help to reduce apparent bulk and create a human scale environment for people where it matters the most - on the street.

#### Guidelines

1. Continuous horizontal walls on a building's street frontage should be 'relieved' by articulating the frontage with contrasting projecting and recessive elements that visually break it up into identifiable parts - this can be expressed further through the use of different materials, patterns and colours;
2. Facades, front and side elevations of multi storey buildings, should visually describe a vertical hierarchy of a base (ground floor), a middle (upper floors) and a top (roof, parapet, cornice, pediment) which is consistent in its expression from the base to the top;
3. Buildings should respond to the horizontal elements of those which are adjacent;
4. Where buildings are built to the side boundary, no windows are to be provided on the side wall. If windows are proposed on a side wall, consideration should be given to the potential for the windows to allow light and air in combination with a side boundary setback to prevent being 'built out' in the future; and
5. Large scale buildings should be considered as a collection of forms rather than one large box form.

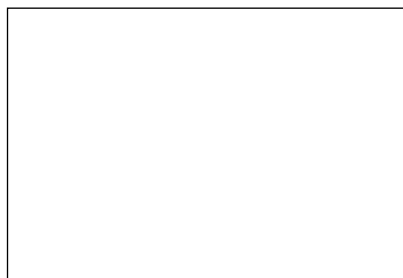
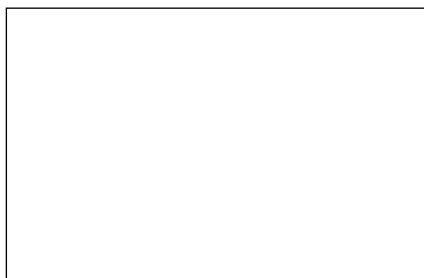
*Guideline Title*

*Guideline Objectives*

*Explanation why the Guideline is Important*

*Design Guidelines*

*Image Examples or Explanatory Diagrams*



### 1.6 Design Guide Area

The Design Guide applies to any new development (except small scale alterations or additions) located within the District Plan Zone “Petone Mixed Use Area” as shown on the map below. This is also described at times as “Petone West” in the Design Guide in reference to its geographic location in the wider Petone context.



## 1.7 Character and Context Description

The existing physical condition of the Petone Mixed Use Area has been influenced over time by its land uses, including industrial, service and retail activities. As this area transforms into a mixed use environment with a different range of activities, the character will progressively change. To assist in understanding the future character planned for the Petone Mixed Use Area, the descriptions below compare and contrast the existing and future character proposed.

### Existing Context

The character of the west end of Petone derives from its history of use and occupation over time. Physical remnants of Maori settlement still exist (urupa and archaeological sites). The whole area and its relationship to the context of hills and harbour remain significant to tangata whenua. As an early settlement of the NZ Company, a formal, regularised street pattern was developed and is still reflected in the grid of streets along the foreshore.

Much of the area was industrialised (meat works, timber mills) in the later 1800's and land was amalgamated into larger blocks, many of which remain. A range of smaller servicing and trade-based activities associated with the larger industrial activities established in close proximity on nearby streets. The housing of workers occurred throughout Petone and the Hutt Valley, with the older housing stock, adjacent to the east of the Petone Mixed Use Area, now popular with people seeking the benefits of the coastal location and amenities of Jackson Street. Petone as a whole has a legacy of older Victorian era architecture, and in the central part of Jackson Street (outside of the Petone Mixed Use Area) many of the original buildings remain. This central part of Jackson Street has become a boutique retail and café location and forms the Jackson Street Historic Retail Precinct. The portion of Jackson Street within the Petone Mixed Use Area is currently dominated by a mix of larger format retail buildings and service industries. These are typically utilitarian structures, make a modest contribution to the streetscape and do not warrant protection.

The railway line went through to the Wairarapa via Petone in 1874, and the Petone Railway Station sits adjacent to the Petone Mixed Use Area providing good public transport connectivity into Wellington City and the wider region. The motorway runs parallel to the rail line. This transport corridor separates the Petone Mixed Use Area from the area further west at the foot of the Korokoro hills. By the late 1980's, most of the larger industrial uses closed down and were being replaced by offices and retail. Today the uses are a mix of older warehouses, distribution, service industries, trade supplies, large format retail (including supermarkets) food related industries and manufacturing.

The built form in the Petone Mixed Use Area generally comprises lower height buildings (1 – 2 stories) with a few exceptions being some taller (up to 8 stories) buildings on The Esplanade and one on Jackson Street. Some of the light industrial buildings in the area, although only one or two storeys high, are relatively large in scale in terms of both height and footprint area. Taller buildings are typically located on larger sites. In the eastern part of the area the sites are relatively small and regularly shaped. The buildings on these smaller sites have a relatively uniform built form, being 1-2 stories in height, setback 5-8 metres from the front road boundary with on-site parking in front, and immediately abutting neighbouring buildings. Vehicle entrances, on-site parking and loading areas dominate streetscapes with limited pedestrian activity. In the block between Sydney and Nelson Streets the light industrial sites abut residential uses.

On the larger sites in the central and western parts of the area, most buildings are 1-2 stories in height reflecting their large format retail and warehouse type uses. These larger sites also have wide expanses of on-site parking and service areas, with buildings typically free-standing within each site. The large sites are serviced from relatively few streets and some informal routes across these larger sites are used by pedestrians and vehicles as a short cut given the limited connectedness available within the transportation network.

The main street network is busy, with high vehicle volumes along The Esplanade and Hutt Road which are on the south and west boundary of the area respectively. Internally within the Petone Mixed Use Area private car/vehicular movements are the predominant mode of transport, along with a high proportion of heavy traffic associated with the industrial uses. There are limited non-vehicular movements (pedestrian and cycling) on the street network.

With the high level of building coverage and on-site parking, close to 100% of the area has an impervious surface. This surface condition generates stormwater runoff ponding issues during extreme rainfall events. There is limited vegetation within the Petone Mixed Use Area, although relatively mature pohutakawa trees exist in several discrete places.



The Esplanade is influenced by the traffic volumes, but with a broader attractive outlook to the beach and harbour. Buildings variously adjoin or are set back from the street edge. Most have no direct ground floor relationship with the street edge.



The streets in the blocks between Victoria and Sydney Streets have small sized sites (average 450m<sup>2</sup>), and many of the buildings are set back from the relatively narrow streets. Frontages are typically utilised for car parking and activities associated with the service/commercial uses. They extend between The Esplanade and Jackson Street so are well located relative to amenities.



West of Victoria Street (above), Jackson Street has an open character with little street edge definition due to car based open parking areas. Some buildings have a direct frontage to the street. The total direct frontage equates to some 40% of the distance between Victoria Street and the Hutt Road.



In the area north of Jackson Street the activities are similar in nature to those in Victoria to Sydney Street block. The buildings vary more in height and type and there is a greater mix of set backs and buildings built to the street edge. The street pattern is less regular than the Victoria to Sydney Street blocks.



Between Victoria and Cuba Street, Jackson Street is a more defined 'mainstreet' space due to the continuous typically 2 storey buildings along its edge. This area is not in Petone West, but is contiguous with it. The heritage values and streetscape character of this section of Jackson Street are important to Petone's identity, vibrancy and its commercial attraction.



On Te Puni Street the urupa (burial ground) remains in use and is the only discrete green open space in the Petone West area. The visual connection from the urupa to the harbour and to the Korokoro hills is important to iwi. Existing buildings encircle the urupa except where it is open to the street.

## Future Context

The desired future for the Petone Mixed Use Area is that it becomes a place to live as well as to establish and operate a business. The change will occur gradually over time and new development will either be interspersed with existing buildings, or large existing open areas will be redeveloped. Over time it is anticipated some of the larger footprint buildings with little value will also be redeveloped. The focus has been on achieving good urban design outcomes for Petone West and references, such as to the NZ Urban Design Protocol, underpin the aims of the guidelines. The aim is for a range of residential living choices and activities to be provided which meet the daily needs of residents and workers. There is an opportunity for housing provision to be a combination of replacement or conversions of existing warehouses and industrial spaces, new apartments and townhouses or terrace houses on smaller sites.



The residential options include (left to right above) adapted industrial spaces/warehouses, new apartments above retail on larger sites developed comprehensively, terrace or town houses on smaller sites.

To successfully encourage people to live in the Petone Mixed Use Area the development quality needs to ensure that incompatibilities (e.g. noise) with existing uses are minimised and that amenity (e.g. open spaces, shelter and connections to desirable destinations like The Esplanade and Jackson Street) is provided for. The interior quality of spaces will need to be considered too, and 'shoe box' spaces will be discouraged.

With a comprehensive form of development planning it is anticipated that new buildings can be built within existing large under-utilised areas, or existing buildings replaced in such a way that the intensity of use and interest increases and the area is used as efficiently as possible.



The comprehensive development of larger blocks of land in Petone West can achieve medium density development for residential and retail uses with open space combined. This will require existing uses to be replaced.

The desired future for the Petone Mixed Use Area is that it has a range of street types that either reflect existing positive qualities, or are streets that are improved by the nature of the new buildings that have a frontage to them. It is anticipated that new streets or lanes may be added to increase diversity and richness within the existing large blocks.

It is important that Jackson Street's character and qualities available within the Jackson Street Historic Retail Precinct are protected to the east, and extended west picking up on building modulation, articulation display windows and verandahs. The development along The Esplanade should take advantage of the outlook, while recognising the traffic constraints that exist.

Some of the smaller sites on the streets east of Victoria Street have the potential for conversions of existing buildings making interesting and intimate street spaces with a mix of service industry and residential development.



The different streets in Petone West can have a form of development which responds to the different qualities. From (left to right) The Esplanade where a set back from the road enables some form of amenity development to occur (this example does not show any street tree planting). The middle image shows potential for west Jackson Street or smaller streets where lower height frontage with activities along its edge can occur. The right image shows potential for smaller streets like Victoria Street, where there may be conversion or replacement of existing buildings to those that have a mix of uses reflective of its 'robust' working history.

It is desirable that development in Petone West provides for employment as well as residential activities. The opportunity exists to diversify the employment base from service, industrial and retail to include more office space. There is also the potential to leverage off the character of Petone to generate spaces that are smaller in scale for use as workshops and studio spaces. It is also desirable to retain the working service industry activities (even if buildings change) that provide part of the character of the area and local services.



There is potential (left to right) for larger scale office buildings (with or without commercial ground floor activities), converted warehouse space type working spaces and retention of existing businesses.

With more open space on site, the amount of vegetation is intended to increase, in terms of the quantity, size, type and quality. The intended result will be a lower proportion of impervious surfaces, with rain gardens and other forms of low-impact stormwater management.

There is an important need for more connections, such as public streets or private accessways within the larger blocks, to provide more richness and diversity of development along with increased frontage. These new connections will encourage a higher level of non-vehicular movement (pedestrians and cyclists) within the area. There is also potential for greater use of public transport given the proximity of the Petone Railway Station. Improving the quality of the walking experience within Petone West will be important to encourage its transition to a mixed use environment that is desirable to live and work in.



## Summary table

ATTRIBUTES	EXISTING CHARACTER	FUTURE CHARACTER
<i>Uses</i>	<i>Service, commercial, large format retail and industrial.</i>	<i>Mix of uses, including existing uses (service, commercial, large format retail and industrial) plus medium density residential and retail (greater than 500m<sup>2</sup>) along Jackson Street. No residential uses on the ground floor along Jackson Street.</i>
<i>Densities</i>	<i>Low with a high amount of open areas with surface car parks.</i>	<i>Increased density of development, including public and private open spaces with a reduced extent of open surface car parks.</i>
<i>Heights</i>	<i>Low, mostly 1 or 2 storeys with a few taller (up to 8 stories).</i>	<i>Mix of low and taller buildings throughout the area. Taller buildings are adjacent to open spaces.</i>
<i>Architecture and Style</i>	<i>Range of eras and no specific styles, utilitarian sheds, simple box forms, basic materials - concrete, corrugated iron, little detail.</i>	<i>Contemporary, more articulation within forms as appropriate to use. More attention to detail at street level, with a range of materials used. Conversions or replacement of existing buildings. Maintenance of existing heritage values in Jackson Street.</i>
<i>Built form</i>	<i>Stand alone buildings with large floor plates mixed with adjoining smaller buildings on streets to the east</i>	<i>Mixed use buildings (retail, servicing, commercial on ground floor and residential and/or commercial above). Some existing buildings remain, but some gaps filled in and other buildings replaced with medium density residential activity and blocks of apartments around private or public open spaces.</i>
<i>Open Space</i>	<i>No green open spaces or public places with amenity except The Esplanade Foreshore</i>	<i>New developments have on-site courtyards or larger open space areas that provide contained sheltered open areas for residents' amenity.</i>
<i>Connections</i>	<i>Large blocks with limited connectivity for all transport modes</i>	<i>Existing streets supplemented by new street connections that generate activity by having additional frontages and better walking accessibility.</i>
<i>Car parking</i>	<i>Large surfaces of carparking fronting the streets</i>	<i>Carparking is located either internally within buildings or behind buildings.</i>

## 2.1 Building Form

Building form refers to the vertical and horizontal scale of a building. Human scale means of a size and dimension that is not dominating to people in the environment around the building.

The objective of managing building form is to generate an environment in which people do not feel overwhelmed by the scale of buildings and so enjoy being there. This in turn leads to more intensity of use, more business opportunities, and makes the area an attractive choice for investment relative to other centres.

Changes in building volumes (height x width x length) and variation in the external walls can reduce the impression of the scale of large developments. Elements such as verandahs help to reduce apparent bulk and create a human scale environment for people where it matters the most - on the street.

### Guidelines

1. Continuous horizontal walls on a building's street frontage should be 'relieved' by articulating the frontage with contrasting projecting and recessive elements that visually break it up into identifiable parts - this can be expressed further through the use of different materials, patterns and colours;
2. Facades, front and side elevations of multi storey buildings, should visually describe a vertical hierarchy of a base (ground floor), a middle (upper floors) and a top (roof, parapet, cornice, pediment) which is consistent in its expression from the base to the top;
3. Buildings should respond to the horizontal elements of adjacent buildings;
4. Where buildings are built to the side boundary, and if windows are proposed on a side wall, consideration should be given to the potential for the windows to allow light and air in combination with a side boundary setback to prevent being 'built out' in the future; and
5. Large scale buildings should be considered as a collection of forms rather than a single large box form.



This building is a new block, but has an articulated form of identifiable parts given recessive and projecting elements (dotted lines). It is typical of the character of older buildings and a useful reference for new buildings.



Horizontal elements consistent with adjoining buildings.

Vertical elements of upper floors consistent with ground floor.



This building has an external facade expressed as a series of forms including a projecting corner form. Various materials and colours emphasise the form variations.



Long blank walls are reflective of utilitarian and internal space requirements. However, these forms can be moderated.

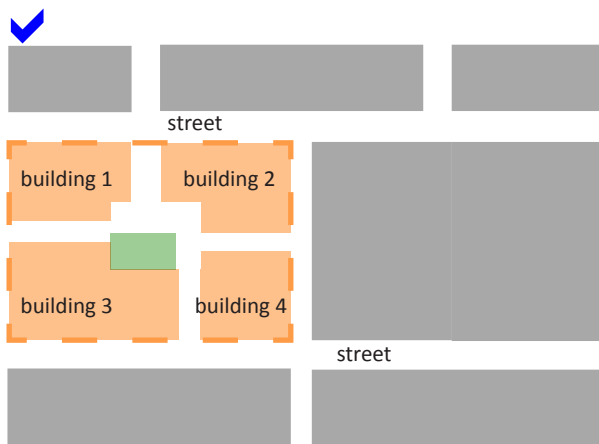
## 2.2 Large Sites

There are a number of large sites in Petone West that should be subdivided over time and redeveloped more comprehensively.

The objective of this guideline is to generate more smaller sites from the currently large sites and so enable greater diversity in development type and uses. This will take time and will most effectively be undertaken in discussion with HCC and other adjacent land owners to make the most of the opportunities to generate new street connections, and open spaces along with a mix of new uses and building forms.

### Guidelines

1. Large blocks and the sites within them should be divided to create new streets, lanes and open spaces in conjunction with new building forms;
2. Reference should be made to the predominant orthogonal street pattern and site sizes in the eastern area when considering the frequency of streets and site size for the larger blocks in Petone West;
3. New development design should be undertaken applying a comprehensive approach to enable such factors as the relationship between building forms and the spaces between them, parking strategies, amenity for building occupants, energy efficiency, staging and other factors to be considered holistically;
4. Consideration should be given to understanding the development aspirations of adjoining site owners as part of a comprehensive design approach; and
5. Early discussion with HCC should be undertaken to look for ways to generate mutual benefits from new street connections, open space and reduced block size.



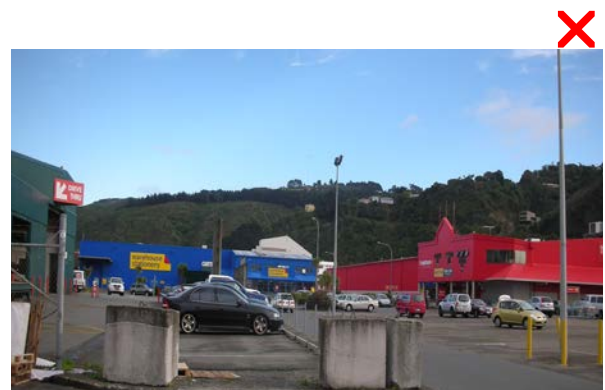
Dividing a large block to create open spaces including streets by the arrangement of building forms.



The sites in this area of Petone West are large and would benefit from mixed use transition by subdivision over time.



New development that retains older buildings and forms new buildings around green open space. The older buildings do not have to be protected for heritage values, but provide a link to history if it is practical to repurpose and retain them.



The large sites in Petone West have developed with large standalone buildings and expansive surface car parking areas.

### 2.3 Prominent Sites

Prominent sites are those which have a higher level of visibility relative to most sites in the area. In a relatively flat landscape such as that which exists in Petone West, these prominent sites are typically viewed along street corridors. The objective for new buildings on prominent sites is to use them as ‘markers’ to assist people’s orientation within and to the Petone West area. The opportunity to generate good development on prominent sites is reliant on the identification of them, followed by owners being encouraged to invest in buildings that are architecturally outstanding and of a quality befitting their visibility.

Prominent sites in Petone West are located at: (a) street ends where the vista down the street terminates with a building; (b) street corners where buildings are visible on at least two sides and they mark a change of direction where one street connects to another; and (c) gateway sites which are usually on a corner, but also where there is a transitional threshold between areas of different character. The plan below identifies the prominent sites within the Petone Mixed Use Area, noting that this plan is indicative of the location of prominent sites and does not reflect their extent.

- Guidelines**
1. Buildings on sites at the end of a street should have a form and facade treatment that responds to the terminating street’s vista;
  2. Buildings on corner sites should visually enhance the change in direction by extending a common facade treatment around the corner; and
  3. Buildings on gateway sites should work in concert with any gateway sites opposite, or comprise elements that are visible at a longer distance, or have a moderated form from high to low, or be of a sculptural form given the potential to be seen from multiple sides.



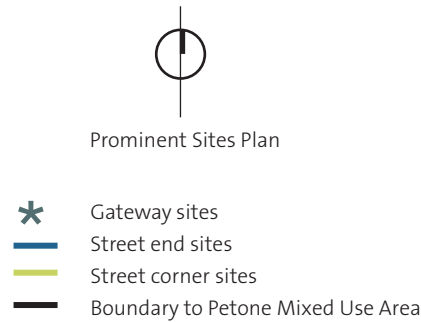
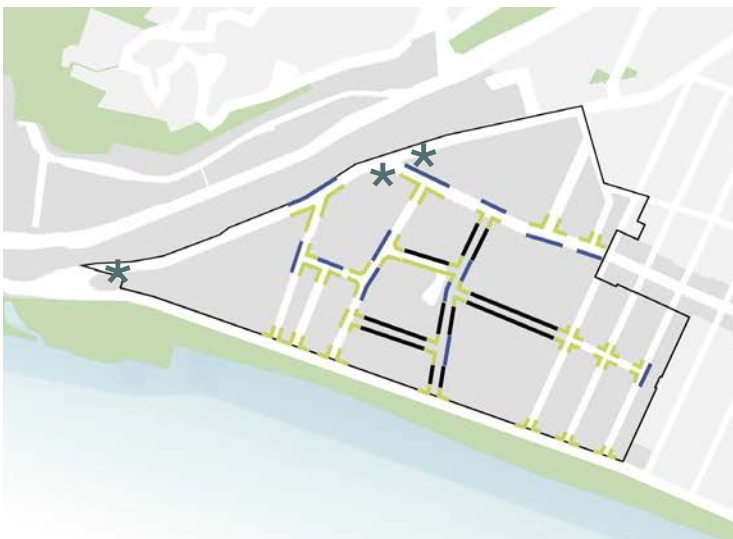
Building at the end of street view: architectural elements (blue facade, windows) are aligned with the centreline of the street



Building at a corner with its facade treatment and rhythm of openings repeating around the corner



Building at a gateway site with a moderated and sculptural form



## 2.4 Street Frontage

The aim of the “Street Frontage” guideline is to encourage existing and new street forms that are interesting and comfortable for people using the Petone West area. This contributes to economic vitality by encouraging people to spend time in the area and enhances the attractiveness to local residents and people from the wider region. It is an objective for Petone West that new streets are added over time.

It is recognised that there are different street types in Petone West and these need to be provided for differently. The guidelines will also seek to ensure that new development in Petone West in proximity to the Jackson Street Historic Retail Precinct (which lies immediately adjacent and east of the Petone West area) is sympathetic to the existing historic character of this area.

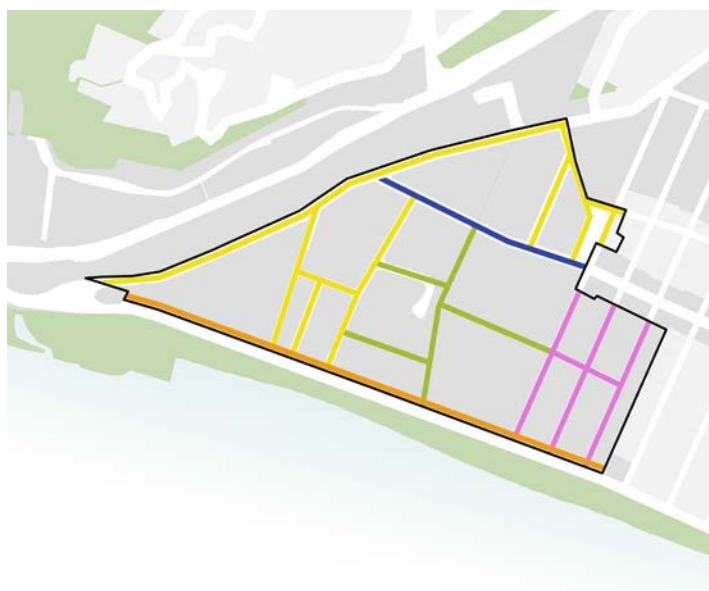
The guidelines for Street Frontage generally seek a positive relationship between the public space of the street and the private space inside the building. It is the position, form and openings in the intermediary front wall of buildings that has the most influence on the outcome of this relationship. Building Bulk guidelines are also relevant. Typically, HCC provides and manages public space while private space is developed and maintained by landowners and their tenants.

Transparent windows, doors facing the streets, lighting, porches and verandahs are all elements that contribute to the attractiveness of streets as public spaces. It is recognised that in Petone West there are a mix of uses and that not all will be able to provide these elements (e.g. warehouses).

The Street Frontage guidelines will address the following:

- A. Continuity
- B. Visual Connections
- C. Entry Elements
- D. The Esplanade
- E. Jackson Street
- F. Residential Development

To recognise the different street types in Petone West (both existing character and desired future character) it is important that they are considered in relation to the “types” set out below. The Frontage Type Table describes the measurable qualities of the guidelines for each type.



Frontage Type Plan

- Type 1 - The Esplanade
- Type 2 - Jackson Street
- Type 3 - New streets
- Type 4 - Victoria to Sydney Street blocks
- Type 5 - Other Streets
- Boundary to Petone Mixed Use Area

FRONTAGE TABLE for STREET TYPES					
GUIDELINE AIM	Type 1 The Esplanade	Type 2 Jackson Street	Type 3 New Streets	Type 4 Victoria to Sydney Streets	Type 5 Other Streets
<i>Buildings front to street boundary</i>	No - but built to set back line of 10m (except on those small sites noted in District Plan)	Yes	Yes	No - but built to set back no greater than 5m	Not necessary
<i>Continuous building across frontage at ground floor</i>	Not necessary	Yes	Not necessary	Yes	Not necessary
<i>Transparent frontage on ground floor</i>	50% minimum	60% minimum	50% minimum	20% minimum	20% minimum
<i>Continuous verandah on ground floor</i>	Not necessary	Yes	Not necessary	Not necessary	Not necessary
<i>Building frontage tenancies at intervals and articulated in form</i>	25m maximum	15m maximum	15m maximum	20m maximum	20m maximum
<i>New driveways, new service lane access or new lane access</i>	Yes	No	Yes	Not necessary	Not necessary
<i>Residential Activities on ground floor</i>	Not necessary	No	Not necessary	Not necessary	Not necessary

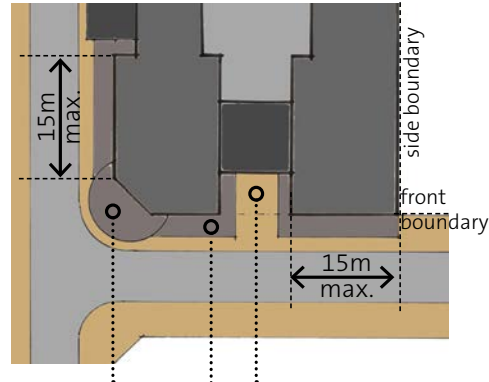
## A. Continuity

The standard aim for town centres is to achieve a relatively continuous street front with visual permeability at the ground floor. A continuous street front also provides 'containment' to the street and assists to make the street a space with higher amenity. It is recognised that in Petone West this will not be practicable on all streets given the mixed use nature of the area.

Although this particular guideline applies mostly to Jackson Street, it is also a good quality that development on other streets can aim for as well, with appropriate provision made for visual connection and building articulation.

### Guidelines

1. Buildings in Type 2: Jackson Street, Type 3: New Streets and Type 4: Victoria to Sydney Streets should have a continuous frontage from side boundary to side boundary at the ground floor;
2. Buildings in Type 2: Jackson Street and Type 3: New Streets should be built up to the street boundary;
3. Corner buildings in Type 1: Jackson Street, Type 3: New Streets and Type 4: Victoria to Sydney Streets should maintain continuity around the corner, as well as be built to the street boundary; and
4. Buildings on Type 2: Jackson Street should have a verandah or shelter attached at between ground and first floor level and extending the width of the site.



Buildings on corner sites should incorporate entrance shelter and windows on both facades

Small setback to shelter doorway and as potential outdoor dining areas

Continuous verandah covers or awnings to make sheltered streets

Plan View: Corner Building on Type 2, 3, 4 Streets-Continuous Frontage which 'wraps' around the corner



Continuous frontage modulated at intervals by change in uses (various shops and restaurants) materials, colours, good use of sheltering elements, high percentage of transparent glazing and public/private lighting



Building faces both primary and secondary street, provides shelter (balcony cover) and visual connection (large proportion of transparent windows on the ground floor with balcony and windows above)

## B. Visual Connection

Windows and doors on the ground floor and balconies and windows on upper floors promote visual connection and interest between the people inside (private space) and outside (public space). The design, location and frequency of openings also contribute to the sense of safety of the users by passive surveillance.

### Guidelines

1. Windows and doors directly facing the streets and open spaces should be provided in accordance with the Frontage Table for Street Types. It is noted that although windows and doors on street edges are not necessarily applicable to all street types, they should be maximised as far as practicable;
2. Blanked out, or false windows and doors, should be avoided in all frontage types. Roller doors should be avoided on Type 1: The Esplanade, Type 2: Jackson Street and Type 3: New Streets, but are recognised as part of the working nature of frontages on other type streets;
3. Opaque windows, reflective windows or solid walls should only be used in the facade where it is below the eye level of people on Type 1: The Esplanade, Type 2: Jackson Street and Type 3 New Streets frontage types; and
4. Buildings in all frontage types should have windows that overlook the street, parks, lanes or pedestrian lanes from any above ground uses.



Reflective windows do not offer visual connection between interior and exterior which is in effect the same as a blank wall.



Non-transparent lower wall section, but at eyeline the windows address the street.



A high percentage of transparent windows provide a good visual connection between inside and outside. This creates visual interest to the passers by and provides opportunities for passive surveillance. These multistorey buildings have above ground spaces that have windows that look out to the street which aids passive surveillance also.





### C. Entry Elements

Well designed, unique and identifiable entry elements, such as awnings, colonnades, feature doors, entrance canopies, porches and verandahs, provide a distinguishable identity, demarcate building entrances, and offer shelter. They also serve to create a transitional space between the inside of the building and the open space outside which can be used for outdoor seating or dining, or for an extension of the internal activities outside in good weather. The entrance should also provide easy access for people with disabilities and parking for cyclists should also be considered.

#### Guidelines

1. Buildings should have recognisable 'legible' entrances by the use of inset setbacks to create sheltered doorways, embayments and porches; or by the extension of canopies out to reflect the use and door-way positioning;
2. Accessibility of the entrance for people with disabilities should be considered;
3. Separate entrances for commercial and residential uses should be provided where the building has both activity types;
4. The entry area should be designed as a transition from the public space of the street to the private or semi public space of the building inside; and
5. The entry area should make provision for bicycle parking and other wheeled vehicles like push chairs or scooters.



Canopies over the street frontage demarcate the use, signal doorways and provide shelter for outdoor seating and dining. Bicycle parking is on the street



This 'opening wall' sends a clear signal ('legibility') as to the entrance way in this modern building and adds visual interest.



The entrance to the building provides covered space for outdoor seating and dining and demarcates the transitional space to the public space of the street



The entrance to the building provides informal seating opportunities and accessibility for people with disabilities.

## D. The Esplanade

Buildings on The Esplanade require special attention regarding their design, placement and function. The appropriate interface between buildings and The Esplanade is fundamental to achieving a quality street. The objective of this guideline is to ensure that developments within the street Type 1 (The Esplanade) create a good physical relationship with The Esplanade and the waterfront so that they benefit from the amenity the area provides. The aim in this area is also to ensure that new developments protect and enhance its openness, natural and recreational values, and visual character when viewed locally as well as from the Hutt Road and SH2. Additionally, The Esplanade is a high volume traffic corridor and limiting the number of vehicle crossings is important to its function as a major through road. Many sites have provision for entrance and egress for vehicles from side streets. It is also recognised that achieving a set back on some sites is unreasonable given their relatively small size and special note is made of these.

### Guidelines

1. Ground floor uses should address The Esplanade with windows and doors;
2. Developments should make the most of the views of the beach and face out towards The Esplanade and beach;
3. New buildings should have a high quality facade and modulated building form to prevent a wall of buildings forming on The Esplanade frontage;
4. A frontage setback area of 10 metres (except for those small sites identified in the District Plan) should be provided and designed in conjunction with the building. This setback space may be used as open amenity space or parking;
5. The front boundary to The Esplanade is to have a minimum of a 2.5m wide planted strip incorporating trees (e.g. pohutakawa trees) capable of growing to 5 metres in height. They should be spaced at no less than 5m centres; and
6. The access for vehicles to each site should occur from side streets so as to not increase the number of vehicle crossings (wherever possible).



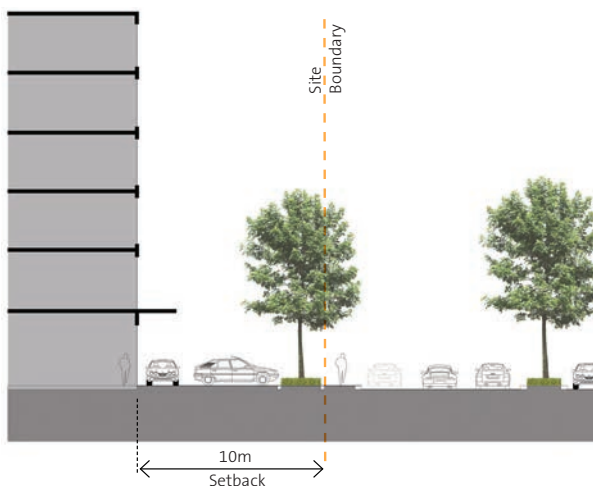
• Building is stepped to avoid noise and pollution from The Esplanade

• Residential tower recessed from the front boundary to improve sunlight access to the street. It is also a good solution to allow for adaptation in relation to The Esplanade in the future.

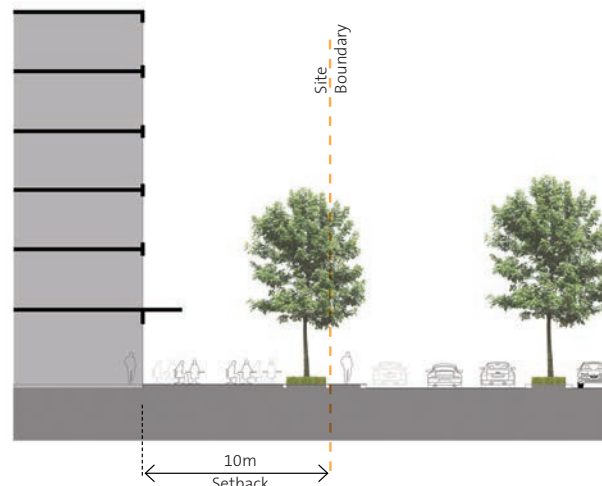
• Rooftop gardens above retail. It can be publicly accessible with restaurants and shops fronting onto it or it can be a communal open space (semi-private) for residents' use.

• Planting provides a green edge to the street.

The placement and frontage set back potential for The Esplanade



The building is setback 10m from The Esplanade. One option is for this space to be used as surface carparking,



The building is setback 10m from The Esplanade. One option is for this space to be used as open amenity space as an extension of the use of the ground floor of the building.

## E. Jackson Street

Jackson Street east of Victoria Street is an important asset for Petone and the Hutt City and is recognised as an historic area in the District Plan. Its reasonably intact grouping of inter-war period buildings with continuous frontages of smaller floor area, typically 2 to 3 storey buildings, in conjunction with street landscape treatment, has generated a setting for speciality shops, and food and beverage businesses. The area is a drawcard for many people who are attracted from outside the area or locally due to its special character. This area east of Victoria Street is not within the Petone West area covered by these guidelines.

It is important that the development of Petone West, both the extension of Jackson Street west of Victoria Street, as well as the area generally, provides a complementary new mix of building types and activities to the historic area of Jackson Street to the east.

The objective of this guideline is to ensure that developments within the street Type 2 (Jackson Street) create an extension of certain qualities of the east section of Jackson Street in the section west of Victoria Street. Also that development in Petone West which interfaces with Jackson Street to the east is undertaken in a complementary way.

### Guidelines

1. Ground floor uses should be commercial/retail, not residential, and designed to address Jackson Street with display windows and doors such that the minimum glazed area is 60% of the front wall area at ground floor;
2. Buildings in Jackson Street should have a continuous frontage from side boundary to side boundary at ground floor level;
3. Buildings in Jackson Street should have a floor area of no less than 500m<sup>2</sup> and tenancies with frontage widths of no more than 15m;
4. Buildings in Jackson Street should be built up to the street boundary, including above ground floors, to a height of no more than 12m and include windows facing the street;
5. Corner buildings in Jackson Street should be built up to the street boundary and maintain continuity around the corner;
6. Buildings in Jackson Street should have a verandah or shelter attached between the ground and first floor level, extending the width of the site;
7. The access for vehicles to each site should occur from side streets and not increase the number of vehicle crossings to Jackson Street; and
8. Buildings in Jackson Street that are adjacent to the existing Jackson Street Historic Area should be responsive to the form of those buildings to ensure the scale is complimentary.



Above ground floors are built to the front boundary with windows facing the street

Continuous verandah frontage

Buildings have display windows to the street

The Jackson Street frontage arrangement for existing buildings to the east of Victoria Street provides a point of reference for the Petone West section of Jackson Street



The building is fronting directly to Jackson Street and has a height of no more than 12m at the front. The height can increase stepped back.

## F. Residential Development

This particular guideline only applies to streets where residential uses are being developed and where the ground floor treatment needs careful design consideration. The objective of this guideline is to ensure residential privacy and passive surveillance to the street. There are other important residential amenity requirements (refer Residential Amenity), but the focus of this guideline is on the street frontage.

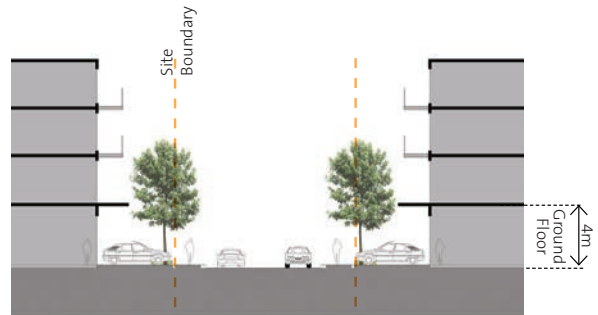
A good interface between public open spaces (streets, lanes and parks) and private spaces is associated with building design that provides passive surveillance (people can see and be seen), a streetscape which is not dominated by garage doors and driveways, and where residents have adequate levels of privacy.

### Guidelines

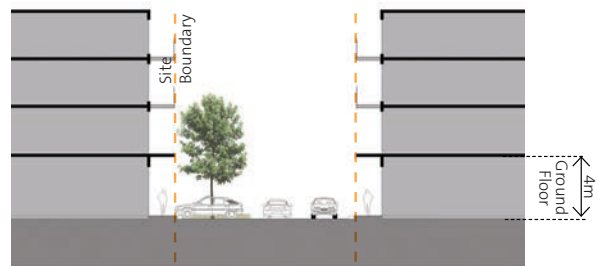
1. No residential activity should be provided on ground level frontages on Type 2: Jackson Street. Buildings on Type 2 streets should accommodate commercial uses on the ground floor. Ground floor residential activities on other street types are not necessary, but where they are provided, the guidelines below apply;
2. Fences fronting public open spaces should have a maximum height of 1.2m. Where fences exceed 1.2m in height above street level, that portion of the fence above 1.2m should be a minimum of 50% transparency;
3. Garage doors should be aligned or preferably recessed from the street front building line;
4. A narrow front yard and change in level of 1.2m is a method that can be used to promote a separation from the public street environment; and
5. Consideration should be given to the use of private open space fronting the street to provide sunlight access and a more open streetscape.



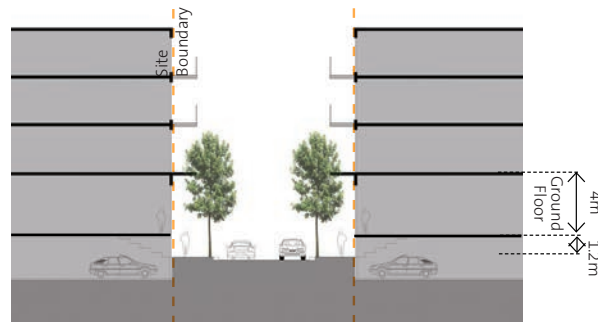
Separation from the street front by set back and height change



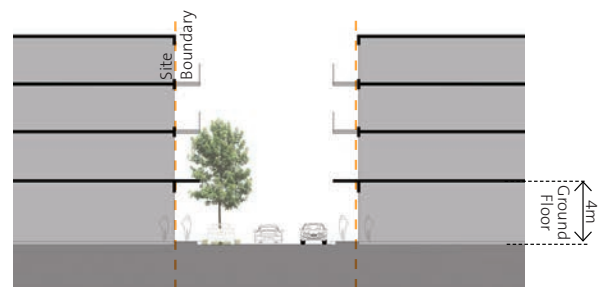
Option for Type 4 or 5 streets: Similar to existing street layout in Sydney Street for example



Option for Type 4 or 5 streets: Buildings are set back 2.5m to allow for a footpath and angle parking.



Option for Type 3,4,5 streets: New buildings are built to the boundary with underground parking and a 1.2m height separation from the street.



Option for Type 3,4,5 streets: New buildings are built to the boundary. Parallel parking on side.

## 2.5 Adjacent Uses

Managing compatibility between new mixed use developments and existing uses is important. The objective of this guideline is to ensure new, potentially conflicting uses are appropriately designed and placed to minimise the incompatibility between them.

This guideline refers specifically to: (a) Buildings adjacent to the Urupa; and (b) Mixed use buildings that abut residential neighbourhoods.

### A. Buildings adjacent to the Urupa

The urupa at Te Puni Street is a culturally significant site on which cultural events continue to take place. Consequently it is important to develop a respectful relationship between the urupa and any adjacent development. Conflict may arise in relation to residential dwelling privacy and overlooking as well as new buildings dominating and shading the site.

The objective of this guideline is to minimise the potential effects of future development on the urupa cultural activities.

#### Guidelines

1. New buildings should be designed with a buffer between the urupa and residential use in order to create aural and visual privacy and reduce potential dominance;
2. New buildings should be designed to provide as far as practicable good visual connections between the urupa, the harbour and the hills;
3. Any windows or open space balconies facing the urupa should have movable screening to enable privacy for the residents from activities in the urupa;
4. New buildings should be designed so as not to dominate the urupa;
5. New buildings should be designed to minimise any shading effect on the urupa; and
6. Any new development in Petone West should consider the potential for other sites of cultural or archaeological significance, including burial sites present below ground, and liaise with HCC in this regard early in the design process.



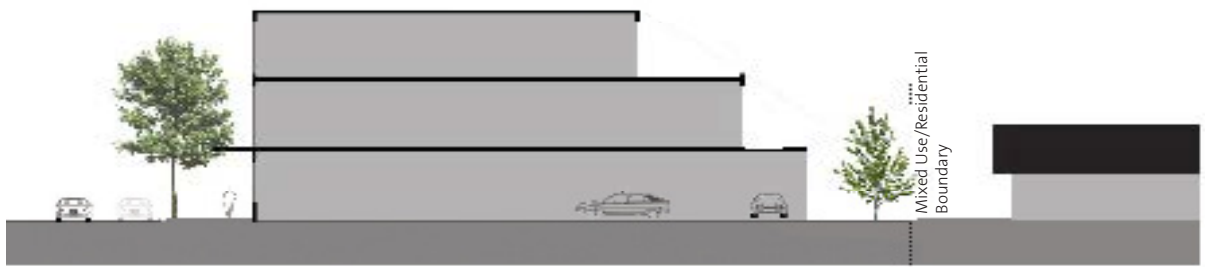
The Te Puni Street urupa is located near to the coast and visual connections from the site to the harbour are important as they are reflective of the very large historic settlement here of Maori and the important relationship the people had with the sea. Similarly, views to the Korokoro Hills are important to the history of the area. The urupa remains in use and activities that occur there can be of a sensitive nature, so compatibility with new adjacent uses should be considered.

**B. Mixed Use buildings adjoining residential neighbourhoods**

The potential effects of mixed use buildings on adjoining residential areas can include dominant, bulky buildings and elongated plain walls. Loss of privacy can occur if overlooking mixed use buildings are placed in close proximity to residential areas.

**Guidelines**

1. A buffer can be created between commercial and residential development. This may be in the form of a landscaped setback and/or open space area.
2. Screens and other devices should be provided to limit overlooking from mixed use activities onto adjoining residential uses; and
3. The scale and massing of new buildings should be sympathetic to adjoining residential uses by applying design devices such as breaking forms into smaller shapes, using stepped back forms, using residential scale shapes and placing windows and open spaces to punctuate facades, and avoiding tall blank walls.



Moderating the effect of scale of new mixed use development on existing residential development can be achieved by a set back from the boundary and stepping back the building form



Adjustable screens enable privacy and management of sunlight access/solar gain to residential development (apartment)



Using creepers, hedges or other dense planting in conjunction with fences and walls to screen residential areas



Using a sloping roof form, building set back and trees provides a transition between the new taller corner building and the existing buildings to the right

## 2.6 Open Space and Greening

Demand for local open spaces for the people working and living in Petone West will increase as the area transitions to a mixed use environment. There is a large open space provided by the Petone foreshore and reserve, as well as by the hills and regional parks to the west. However, the area would benefit from smaller parks or plaza spaces to provide for seating, play and general amenity. New uses such as residential also benefit from some on-site private open space.

The objective of this guideline is to assist the delivery of good quality open space within Petone West in association with private development, and to ensure that open space dedicated to each dwelling unit is of a reasonable quality. This guideline refers to:

- A. Residential Outdoor Areas.
- B. Public Open Space
- C. Greening

### A. Residential Outdoor Areas

The “Residential Outdoor Areas” guideline aims to enhance the urban amenity for residents by providing suitable private outdoor areas. The demand for good public, semi-public and private open space is likely to increase with the higher population and land uses densities intended in the future. This guideline seeks provision of on-site open spaces.

#### Guidelines

1. Residential developments should provide outdoor areas which can be in the form of private and/or shared open spaces;
2. Internal or lane type circulation space also provides open space amenity for residents;
3. Outdoor open spaces should be located where they will receive sunlight and be of a dimension that residents can functionally use; and
4. Privacy should be provided for by incorporating planting and/or external devices such as louvres, shutters and blinds where required.



Internal courtyard type shared space provides places for play and provides light and air circulation to residential uses.



Balconies as private outdoor areas in residential apartments or townhouses.



Internal courtyard type shared space provides places for play and provides light and air circulation to residential uses.

**B. Public Open Space (Encouraged)**

A high quality and usable open space is safe, active, convenient, well maintained, pleasant, connected and appropriate to its context. An unsuccessful space is the one that disregards the existing network of open space and the connections to the pedestrian and cycle routes, wind and solar aspect and the size, location, and nature of the activities pertinent to the site and its surroundings. An unsafe open space is one that does not consider the uses and interface of the buildings fronting it and creates hidden, inconvenient, unattractive, poorly maintained and unlit spaces.

The design of open space should be integrated into the urban design and building design process. Provision of public space will necessitate discussion with HCC to ensure that it is strategically well located and is designed to appropriate standards. Open spaces can be in the form of green parks, hard surface plazas, courtyard or pocket park scale spaces. In Petone West it is considered that some form of containment is beneficial to provide sheltered spaces as an alternative to the nearby large open spaces on the Petone foreshore.

**Encourage Guidelines**

1. Provision and design of public open space should be developed in relation to local demand. Consider if the space is to be used by workers during lunch time (seating places, lunchtime sun, shading) or by residents (playground, seating places, “kick and play”);
2. Consider the relationship and function of the proposed open space relative to other open spaces in the area with the aim of each space contributing over time to a network of different spaces that can function and respond in different ways to use needs and climatic conditions;
3. Open space intended for public use should be located where it is accessible (on main walking and cycling routes), highly visible (“eyes on the streets”, visual linkages and no hidden spaces), enables through traffic use by pedestrians and cyclists (footpaths and cycleways) and within walking distance of the catchment of core users;
4. Open spaces should be in locations and any associated buildings designed to ensure the open space receives a minimum of 2 hours of sun per day from 12pm to 2pm during mid winter;
5. The ground floors of buildings fronting open space should have pedestrian-oriented active uses (preferably retail and community uses);
6. The selection of trees and plants should be appropriate to the location, type and scale of the open space and its use, recognising for example climatic influences, maintenance requirements, irrigation regimes, and mature height of trees;
7. Outdoor lights should be provided, preferably attached to an adjacent building façade, and be of a type to a public space and human scale; and
8. Residential development should, in addition to any public on-site space, provide on-site open space in the form of private areas .



The location of new open space should be considered in conjunction with building development. Petone West needs a series of sheltered and contained open spaces that will provide a range of options for uses at different times and in different climatic conditions. Large open spaces with no shelter will have less amenity.



### C. Greening (Encouraged)

“Greening” guidelines address how development can create better amenity in the Petone Mixed Use Area. The emphasis will be on initiatives to provide spaces such as rooftop gardens and green walls. The objective of this guideline is to promote aesthetic improvements to the urban environment, as well as to assist in increasing biodiversity, reduce the heat island effect, enhance indoor and outdoor air quality, and reduce water usage by adoption of efficient water management systems.

#### 1. Rooftop Gardens

Rooftop gardens (intensive green roofs) are typically terraces or areas on the top of a building that can include paving and usually grass, trees and shrubs. They provide usable outdoor areas, have good insulation capabilities and can assist with stormwater management.



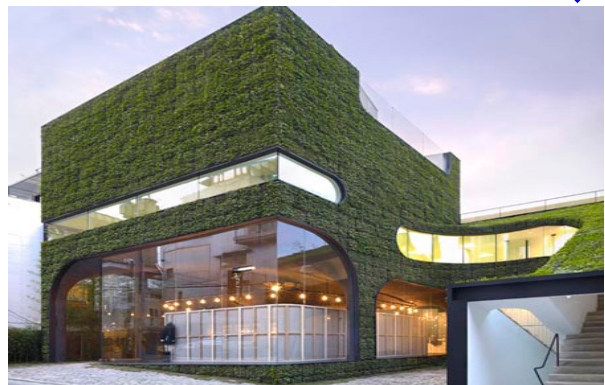
Green roof and balcony

#### 2. Green Roofs

Green roofs (extensive green roofs) consist of a vegetated roof area and are not designed as usable amenity spaces. They assist in increasing biodiversity and insulation capabilities, reducing water usage through adoption of efficient water management systems.

#### 3. Vertical Green Treatments

Green walls, green indoor or outdoor atrium and landscaped balconies are all part of the vertical green spaces initiative. They are methods to aesthetically restore urban environments (visual relief for blank walls and tall buildings) and control noise pollution (soundproof capabilities).



Greening of building facade

#### Encouraged Guidelines

1. Roof gardens and vertical green treatment is encouraged. The spaces created can either be publicly accessible (e.g. as part of the open space network), semi-public (e.g. for residents of a building) or not for use (design feature). If green roofs, roof gardens and vertical green treatments are to be used they should consider:

- waterproofing, drainage systems and structural strength to support weight loadings;
- plant species that are resistant to severe environments (wind and drastic changes in temperature), require low maintenance and low water use;
- soil mix and depth;
- maintenance procedures and access;
- opportunities to use collected rainwater for irrigation; and
- plant types that maximise solar access in winter and control solar infiltration in summer.



Green wall grown from base on facade framework

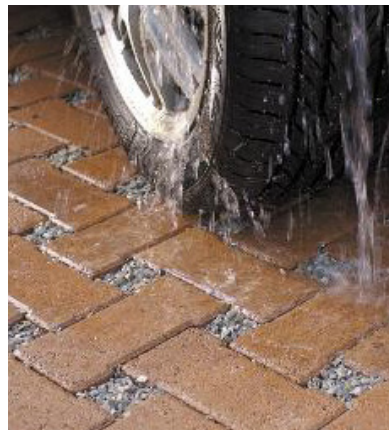
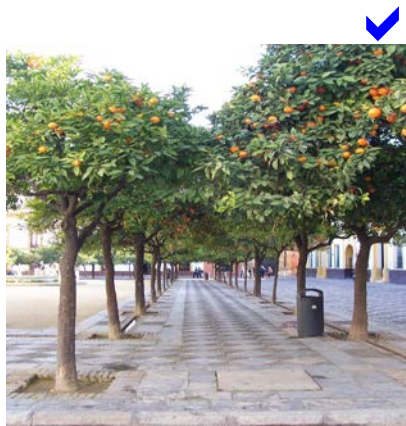
## 2.7 Stormwater (Encouraged)

The Petone West area is sensitive to stormwater volume in some places as the existing stormwater network has limited capacity to accommodate peak events and discharge to the sea. It is considered good practice to sustainably manage stormwater on site as much as practicable.

The objective of this guideline is to reduce stormwater runoff, integrate stormwater management into the overall design and consider future maintenance of stormwater devices.

**Encouraged Guidelines**

1. Low impact design practices should be used to encourage stormwater infiltration and reduce runoff. Impermeable surfaces should be minimised to reduce the volume of stormwater runoff and consideration given to treating stormwater runoff with swales and rain gardens;
2. Minimise the development impact on stormwater quality and generation, and incorporate water efficiency initiatives at the design phase. Consider rainwater collection from roofs for watering gardens and flushing toilets; and
3. Plan for the future management of devices to ensure ongoing effectiveness such as access for cleaning and replacement of elements.



Stormwater detention can occur by the use of storage areas as part of the landscape, or tanks can be used to collect roof runoff and reuse for services within the building or for irrigation purposes

Stormwater devices can be incorporated into the landscape design of new developments.

It is important to minimise impervious surfaces by using porous paving as much as possible to reduce to stormwater runoff

## 2.8 Car Parking

The Petone West area has extensive areas of on-site car parking. Although vehicle parking is essential to enable people to gain access to the employment and commercial activities in the area, the design and extent of surface car parking is detrimental to the quality of the environment as a mixed use area. Provision of parking as part of a structure is a preferable alternative. However, this must be carefully positioned to avoid adverse effects on the street frontage.

As the area transitions to provide for more diversity and richness in its built form, open parking areas will provide opportunities for future development. New parking can also be provided which is of a quality that improves on that of existing areas. This guideline is divided into two parts: (a) Locating on-site car parking within a building; and (b) Addressing surface car park scale and amenity.

### A. Locating On-Site Car Parking within a Building

Car parking structures, if not designed with consideration as to their effects, can dominate the streetscape with poor quality frontages and loss of passive surveillance. This objective of this guideline is to encourage parking structures to be provided that do not compromise street amenity.

#### Guidelines

1. Car parking within buildings should avoid directly fronting to streets or parks at ground level. Parking should be placed back into a building structure so as to be enveloped by associated active residential, commercial or other uses;
2. Car parking above first floor (ie above a ground floor retail/commercial/residential tenancy) should also be within a building structure so as to be enveloped by active uses. However, if this is impracticable, the building should incorporate design features such as green walls and screen devices to minimise the visual impact of parking floors on public spaces;
3. Car parking within smaller scale building developments, such as for townhouses or terrace houses, should be located in garaging (or surface parking) at the rear of the building with access via a service lane or access from the street; and
4. Car parking within smaller scale building developments that incorporate a garage with direct access from the street should have a garage frontage that is less than 50% of, and set back from, the frontage of the building.



Car parking garage is integrated within medium density residential building and sits back behind a street facade. Garage is a relatively small part of the frontage and is set back from the front wall



Green walls are a good solution to minimise the negative visual effects of car parking above ground within a large scale structure.



Car parking structure dominates the streetscape; inactive street edge and elongated blank walls.

**B. Addressing Surface Car Parking**

Large areas of open surface car parking can have a detrimental impact on the visual appeal of an area and reduce the comfort and safety of pedestrians and cyclists. This guideline aims to ensure surface car parking is provided in a way which mitigates the detrimental impact on amenity through the use of such measures as planting and attention to the scale and location of parking area placement. Smaller sites providing surface car parking can also adversely affect the quality of the public environment if it is poorly located and dominates the streetscape.

**Guidelines**

1. If surface car parking is provided, it should be located behind buildings for street Type 2: Jackson Street and Type 3: New Streets;
2. Surface parking fronting street Type 1: The Esplanade, Type 4: Victoria and Sydney Street blocks, and Type 5: Other Streets should not exceed more than 50% of the total site frontage;
3. Surface parking for development (that cannot be accommodated where it is allowed on a street frontage [Types 1,4,5]) should be located at the rear of the building;
4. Trees and planting should be located to alleviate the negative visual effect of car parking fronting public spaces and buildings, and to provide shading for cars and pedestrians;
5. Surface car parking should incorporate 1 tree per 4 parking spaces along with low water use and low maintenance shrubs; and
6. Porous pavement such as permeable pavers, permeable concrete and permeable asphalt should be used wherever practicable.



Large surface of car parking fronting the street; poor landscaping - not enough trees and shrubs.



Shading structures, trees, shrubs and appropriate paving minimise the visual impact of large surface car parking area. Areas of porous paving and vegetation are encouraged to reduce urban water runoff.



Surface car parking at the rear of terrace houses with landscaped courtyard setting.



Surface car parking at the front of terrace houses is not dominant

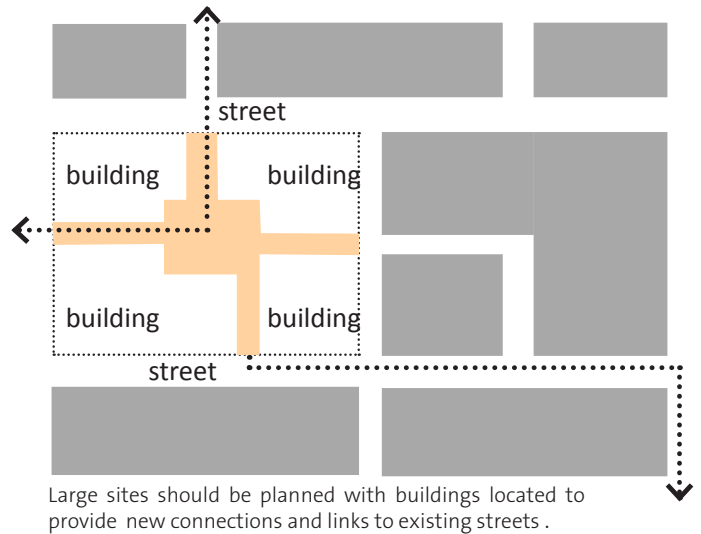
## 2.9 New Streets (Encouraged)

Mixed use areas should have lively street environments generated by a wide range of activities. In Petone West, new streets are encouraged to be formed in association with new development on larger sites, particularly within the large street block formed by Te Puni Street, Jackson Street, Victoria Street and The Esplanade. The objective of this guideline is to ensure that new streets are located and designed to generate positive streetscapes and enhanced pedestrian amenity.

The plan below illustrates the existing street network in the Petone Mixed Use Area and suggests where improvements can be made through the addition of new streets. These improvements are indicative only and the location and addition of other streets will require specific design consideration. Opportunities for new streets to improve the accessibility and connections through the area can be realised from the future development of larger blocks.

### Encouraged Guidelines

1. New streets should assist with the permeability of existing large blocks by creating new through-street linkages;
2. New streets should connect into the existing street network;
3. Developments which front new streets should reflect Street Frontage Type 3: New Street conditions;
4. Paving, lighting, landscaping, servicing and the interface with buildings should be carefully considered;
5. New streets should be designed to facilitate pedestrian, vehicle and cycle movements; and
6. New streets should be designed to reflect the volume of traffic use. Smaller scale streets and lanes are encouraged to provide a 'hierarchy' of streets within the area.



Frontage Type Plan

- Type 1 - The Esplanade
- Type 2 - Jackson Street
- Type 3 - New streets
- Type 4 - Victoria to Sydney Street blocks
- Type 5 - Other Streets
- Boundary to Petone Mixed Use Area

### 2.10 Wind

The objective of this guideline is to minimise the adverse effects of wind on streets and public places to create a more sheltered, safe and comfortable mixed use area in Petone West.

Petone is at a coastal location and in a valley and there is no expectation of eliminating wind from outdoor locations. The intention is to guide the orientation, massing and form of buildings to influence the wind conditions at the ground. Some of the negative effects of building design on wind flow are:

- Streets that present significant variation of building heights (a taller building adjacent to a shorter one) can exacerbate adverse wind conditions;
- Taller buildings that create increased wind speeds at the ground level (downwash effect);
- Horizontally elongated plain façades (a building that is short but long) that can have a detrimental impact on pedestrian discomfort due to increased wind speeds (“row” effect); and
- Alteration or demolition of buildings can change wind flow pattern and speed at ground level and affect neighbouring buildings by funneling wind in.

**Guidelines**

1. New buildings should be designed with reference to the existing wind patterns of the site and not increase wind speed at ground level;
2. Projecting and recessive elements (such as balconies, verandahs, set backs) should be used to reduce the adverse effects of wind at street level; and
3. Consideration should be given to generating sheltered open spaces which respond to the predominant wind directions.



Windy streets are uncomfortable and can be dangerous in high wind speeds. This discourages their use as public spaces.



projecting roofline reduces wind flow down the face

recessive building elements reduce downwash

balconies break downwash and wind corner speed

verandah shelters footpath from down wash

articulation by recessive elements in ground facade limits row effect



## 2.11 Residential Amenity

### A. Gaining Solar Access (Encouraged)

Designing for solar access means providing opportunities for the sun to penetrate a building or an open space to gain solar heat in winter and control solar radiation in summer. Solar access is commonly differentiated between “passive solar access” and “active solar access”. The objective of this guideline is to encourage good solar access to new developments in order to reduce the energy required for heating in winter and cooling in summer.

#### 1. Passive Solar Access (Encouraged)

Addressing the local climate and solar aspect of a site and applying simple design and building techniques can lead to energy use reductions.

North facing windows receive more of the sun’s heat in winter (sun is low in the sky) and less in summer (sun is high in the sky). East and west facing windows normally receive more sun in summer and should be minimised as they can cause a building to ‘overheat’.

High thermal mass materials (such as concrete) are very important due to their capability to absorb and store heat gained during the day which is then released to keep rooms warm into the nights. Windows should be carefully placed and sized as they can easily let heat in, but can also release heat out at night much faster than insulated walls.

#### Encouraged Guidelines

1. Windows of residential living areas and bedrooms, commercial, retail and community uses facing north are encouraged;
2. Sunlight access through the roof is encouraged when north-facing windows are not possible (skylights or clerestory);
3. Horizontal shading devices are encouraged on north-facing windows (awnings or overhangs);
4. Materials that have high heat-storage capacity such as stone, brick and concrete are encouraged, especially on north-facing walls;
5. Building depths of no more than 15 metres are encouraged to allow cross ventilation and natural daylight into internal spaces. 10m to 13m deep buildings can be naturally lit and ventilated. 14m to 15m deep buildings may require some artificial ventilation and lighting; and
6. Placement of windows that maximise natural cross ventilation is encouraged to reduce the need for air conditioning during summertime.

#### 2. Active Solar Access (Encouraged)

Active solar-thermal systems are solar collector devices (generally solar hot water systems) that capture sunlight and transform it into energy. The solar energy gained can be either transferred to supplement hot water heating or space heating, or it can store excess heating generated by the collectors for future use. Solar photovoltaic panels use solar cells to capture the sun’s energy and convert it into electricity for lighting, heating and powering equipment.

#### Encouraged Guidelines

1. Install solar hot water systems and/or photovoltaic panels;
2. Consider the effective orientation and inclination of any active solar system to maximise sunlight absorption;
3. Consider collectors that can track the path of the sun rather than those on a fixed mounting to increase solar heat capture; and
4. Consider the visual impact of active solar systems.

Solar collector devices positioned to maximise sunlight absorption



Eaves and moveable vertical shading device (external venetian blind) on east or west facing windows



Horizontal Shading device (overhangs) on north facing windows



## C. Solid Waste

The objective of the Solid Waste guideline is to ensure the provision of well screened and conveniently located on-site rubbish storage areas. Storage for rubbish bins can involve bin spaces for individual dwellings or retail tenancies, or communal rubbish storage for multi-dwellings or commercial buildings.

### Guidelines

1. Solid waste containers or storage areas should be screened from public spaces and be located away from the building front façade;
2. Solid waste should be conveniently located to facilitate use and collection and appropriately sized to meet user needs; and
3. Solid waste storage should be appropriately ventilated to avoid odours that may affect adjoining activities.



Storage bins exposed to public view



Storage bins screened from public spaces

## D. Noise (Encouraged)

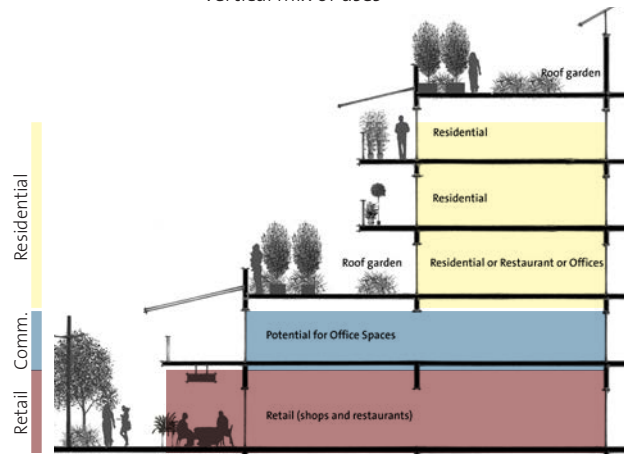
This guideline complements the mandatory noise requirements of the District Plan. Its objective is an urban environment where adverse noise effects are minimised. Although a mix of uses in the Petone West Area is desirable there is potential for some uses to have a detrimental noise effect. The noise of evening activities such as restaurants, cafes and bars or community activities can disturb residents living above or adjoining. Similarly, attenuation in mixed use buildings is desirable to provide noise effect mitigation to residents from co-located commercial activities.

### Encouraged Guidelines

1. The potential noise context of the site (incl. noise sources outside the building) should be considered in the building design; and
2. Acoustic design should address potential for internal noise (between tenancies or units) including through the insulation of floors and walls, but also in the arrangement of uses within the building.



Vertical mix of uses



Vertical mix of uses

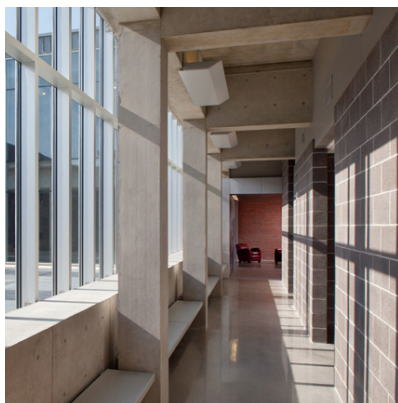


## E. Quality of Internal Residential Space

The objective of this guideline is to encourage good quality residential development in Petone through careful attention being directed towards the design of internal spaces. To enhance Petone West's reputation as a good place to live, it is important that the quality of internal spaces is considered with the other guidelines that apply.

### Guidelines

1. Developments that include residential dwellings should have appropriately proportioned spaces that enable comfortable use by the occupants including habitable rooms (dining, living and bed rooms) that have a minimum dimension in any one direction of 2.8m;
2. The minimum floor to ceiling height in habitable rooms is 2.7m between finished floor level and finished ceiling level;
3. Habitable rooms should have windows to the outside and the living area should have direct access to private outdoor space measuring not less than 2.5m<sup>2</sup>;
4. All residential activities should provide a minimum area of 20m<sup>2</sup> per residential unit as either private or shared outdoor amenity space;
5. Design proposals should demonstrate how habitable rooms within each dwelling are provided with an adequate level of privacy in relation to neighbouring properties, the street and other public spaces;
6. All developments should provide optional (ie the residents can choose to utilise it) storage space for cycles and other equipment; and
7. The number of dwellings accessed from a single corridor should not exceed 8 per floor, and the corridor should receive natural light and adequate ventilation.



Corridors should be light and limited in the number of dwellings they provide access to.



Dwellings should have a reasonable head height, not be long and narrow and have natural light.



Storage for the likes of bicycles and other equipment should be provided for.

## 2.12 Adaptation & Resilience

The objectives of this guideline are to design to: (a) address the need for flexibility so that new buildings or good quality existing buildings can change and adapt in the future in a way that ensures their character attributes are maintained, or alternatively lesser quality existing buildings can be adapted to make a positive contribution to the area without the need for complete demolition; and (b) recognise and manage development so it has an increased level of resilience to natural hazards in Petone West.

### A. Design for Adaptation and Change

Traditionally, buildings were constructed with relatively high floor-to-ceiling heights and good quality materials that had a long lifespan. These attributes, combined with their modular structures and layouts, facilitate an easier conversion of old buildings into new mixed use developments. The objective of this guideline is to encourage new buildings to be designed to enable greater flexibility of use and adaptation to respond to different needs that may arise in the area over time.

#### Guidelines

1. A minimum of 3.4 metres of floor-to-ceiling height should be provided on ground floors, and a minimum of 2.7 metres of floor-to-ceiling height should be provided on upper floors;
2. In residential or commercial developments, the provision of separate entrances to ground and upper floors should be provided;
3. Building depth between 10 and 15 metres should be provided to maximise adaptability between residential and commercial uses and to provide for natural light and ventilation;
4. Additions and alterations should respond to any positive attributes of the character of the building or area, including any heritage values associated with adjacent buildings; and
5. Extensions or alterations to existing high quality buildings should be in harmony with the old structure and should not dominate the original building.



The existing large floor plate warehouse type structures in much of Petone West, or the industrial nature of smaller buildings can be adapted to new uses. Retaining the area's industrial character will give it identity and interest.

### B. Natural Hazards

The primary hazard identified in Petone West is the fault line which runs through the area approximately parallel to the Hutt Road (this is identified in the District Plan). This fault could cause ground shaking, ground rupture and liquefaction and the District Plan has provisions in this respect. The area is also vulnerable to sea level rise. The natural hazards occurring in the area have an impact on current and future development. It is possible to manage the location and design of buildings to respond to the potential adverse effects of natural hazards on human life, property and the environment.

#### Guidelines

1. Building design should appropriately respond to the seismic risk the area presents as well as for potential sea level rise whilst considering implications for the other urban qualities the guidelines seek; and
2. A precautionary approach should be adopted in relation to use or development affected by potential natural hazards.

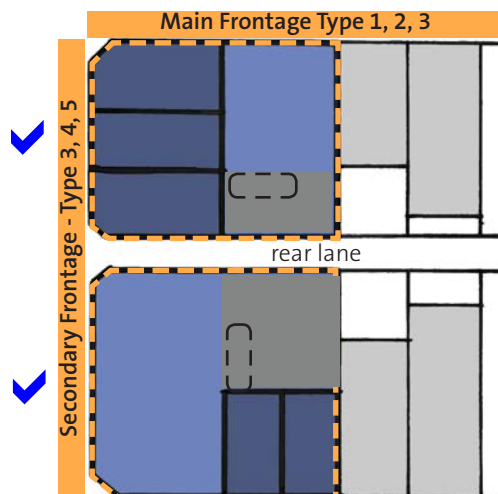
## 2.13 Large Format Retail

Well designed large format retail developments can make a contribution to the attractiveness and vibrancy of Petone. However, they can include elements such as featureless walls on street edges, bulky buildings, inactive street frontage, and large surface of car parking fronting the streets which adversely affect the quality of the place. It is important that developments integrate with the traditional urban fabric of the area and apply good urban design techniques to assist in maintaining and enhancing pedestrian amenity and safety.

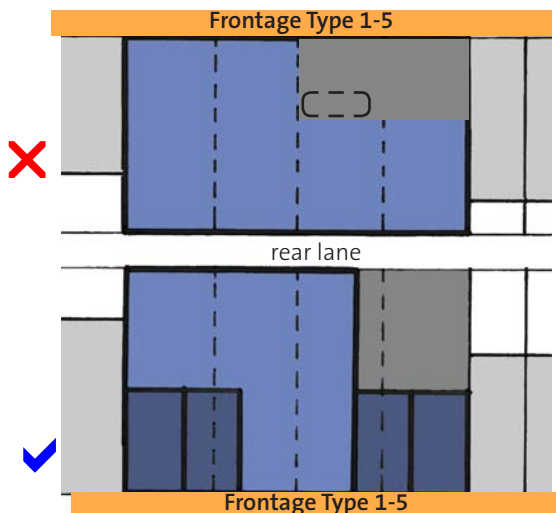
The objective of this guideline is to ensure that new buildings or alterations to large format activities contribute to the intended future character of the area (refer to 1.7 Character and Context Description).

### Guidelines

1. The composition of activities along street frontages (e.g. set backs and frontage lengths) should recognise the type of street described in Guideline 2.4;
2. Where a proposed development has a frontage exceeding 100 metres in width, a through block pedestrian link or new street should be provided;
3. The utilisation of new streets or lanes as described in Guideline 2.9 should be provided to enable access for parking at the rear or for service such as loading; and
4. Car parking should be provided with reference to Guideline 2.8.



Examples (above and below) of large format activity fronting streets (indicative only). Example above of new supermarket built to corner and with smaller tenancies at the edges. Note that in Petone West the minimum size for smaller tenancies of retail is 500m<sup>2</sup>



- Larger format activities - retail permitted 500 - 3000m<sup>2</sup>
- Smaller scale activities - retail not permitted below 500m<sup>2</sup>
- Location of surface car parking
- Location of service / loading area
- Adjoining sites
- Corner site

### 3. Appendix

### 1.3 How the Design Guide Relates to the Petone Vision

The Design Guide has been prepared taking into consideration the elements contained in the Petone Vision to 2027. Table 1 below describes the vision elements and corresponding actions, and references the parts of the Design Guide which can assist to give effect to them. It is important to recognise that:

- The Petone Vision applies to the whole Petone area, not just the area covered by the Design Guide. Accordingly, in applying the vision to Petone West, the variation between the areas is important.
- Some of the existing characteristics of Petone West (refer to section 1.7) influence and, to an extent, limit what is reasonably possible for its future and the Design Guide needs to be cognisant of these characteristics.
- The Petone Vision includes elements which cannot be achieved by the Design Guide and the District Plan. Similarly, the District Plan and Design Guide cannot be expected to single-handedly achieve the vision as there are many other influences on the future shape of the Petone West area.
- Many of the Vision elements and actions relate to ensuring the qualities of Petone are not adversely affected by change. There are some qualities in Petone West that are positive and these should be retained (e.g. urupa). However, most of its current quality is poor and the provisions of the District Plan and the Design Guide seek to change them.

**TABLE 1: VISION TO DESIGN GUIDE REFERENCE**

Element 1: A distinguishing feature of Petone is it being a unique heritage place. This means:	Design Guide Response
celebrating, preserving and promoting the heritage aspects that make Petone’s heritage and character distinctive;	[2.4E] Jackson Street [2.5A] Adjacent Uses (urupa)
ensuring change is sympathetic and reinforces the heritage look and feel (in particular around Jackson Street and adjoining streets);	[2.4E] Jackson Street [2.5B] Adjacent Uses (abutting residential) [2.9] New Streets
recognising the nature and scale of the urban fabric around Petone and the residential areas surrounded by harbour, river, parks and hills;	The Design Guide has no direct influence on the nature and scale of the urban fabric around Petone
re-establishing and celebrating Iwi links to important sites (taonga); and	[2.5A] Adjacent Uses (urupa)
protecting, and celebrating the heritage and cultural roots of Maori and settlers;	[2.5A] Adjacent Uses (urupa)

**Element 2: Growth in Petone will be managed in an economically and environmentally sustainable manner through:**

**Design Guide Response**

proactive management, planning and investment for Petone’s future prosperity;

Guide cannot directly make Petone prosperous. But by communicating future design expectations it may influence investment.

initiatives to address environmental qualities of Petone, including heritage;

- [2.4E] Jackson Street
- [2.5A] Adjacent Uses (urupa)
- [2.6] Open Space and Greening
- [2.7] Stormwater
- [2.10] Wind
- [2.11] Residential Amenity

increased opportunity for residents to work locally;

Guide cannot directly make work opportunities. But by communicating future design expectations it may influence investment in new business and residential activity. With the changes sought there may also be change in business as the area transitions to a mix of uses.

more local businesses working in a supportive environment;

Guide cannot directly make more business opportunities or the supportiveness of the environment. But by communicating future design expectations it may influence investment in new local business. The Guide seeks more street frontage, both of which are aimed at increasing the ground level business opportunities.

increasing the attractiveness of walking, cycling and public transport options;

- [2.4] Street Frontage
- [2.6] Open Space and Greening
- [2.7] Large Sites
- [2.9] New Streets

sympathetically achieving a wider range of housing choice;

- [2.4E] Street Frontage (residential)
- [2.5B] Adjacent Uses (abutting residential)
- [2.6] Open Space and Greening
- [2.11] Residential Amenity

carefully managing any increasing population in Petone;

Guide cannot manage population.

supporting investment with attention to design quality that reinforces and enhances Petone’s character; and

Guide’s purpose is to manage design quality. As noted above the character of Petone West is proposed to change.

changed roading networks that improve the movement of residential and business traffic and add amenity value to areas such as the foreshore.

- [2.4] Street Frontage
- [2.6] Open Space and Greening
- [2.7] Large Sites
- [2.9] New Streets

<b>Element 3: We recognise that Petone has to be a real place for our people. This means:</b>	<b>Design Guide Response</b>
ensuring Petone is a safe community;	[2.4] Street Frontage [2.6] Open Space and Greening [2.9] New Streets [2.10] Wind [2.12] Resilience (natural hazards)
encouraging diversity as a strength within an inclusive community;	Guide has no influence on the diversity or inclusiveness of the community.
continuing to recognise and support the suburb of Korokoro and its contribution to Petone’s overall wellbeing; and	Guide has no influence on Korokoro.
having a strong sense of community and continuing to meet local needs locally.	[2.4] Street Frontage [2.6] Open Space and Greening [2.9] New Streets
<b>Element 4: Petone needs an attractive and vibrant village culture at its heart. This means:</b>	<b>Design Guide Response</b>
being recognised as family friendly;	[2.6] Open Space and Greening [2.11] Residential Amenity
catering to changing needs and diversity within our community;	Guide has no influence on the diversity of the community.
enhancing and being encouraging of employment and business as a key contributor to vibrancy;	Guide cannot directly influence employment and business. By communicating future design expectations the guide may influence investment in new business.
retaining small scale commercial activities and retailing as a defining feature of Petone and the Jackson Street character;	[2.4] Street Frontage [2.6] Open Space and Greening [2.9] New Streets
developing a cultural heart to support creative arts;	Guide has no influence on the ‘cultural heart’ of the community.
fostering quality design for all private property and public space development;	Guide’s purpose is to manage design quality.
engaging with the harbour, hills and river; and	[2.4D] The Esplanade
recognising and supporting the important role of learning institutions within Petone, including life long learning.	Guide has no influence on the learning institutions of Petone.

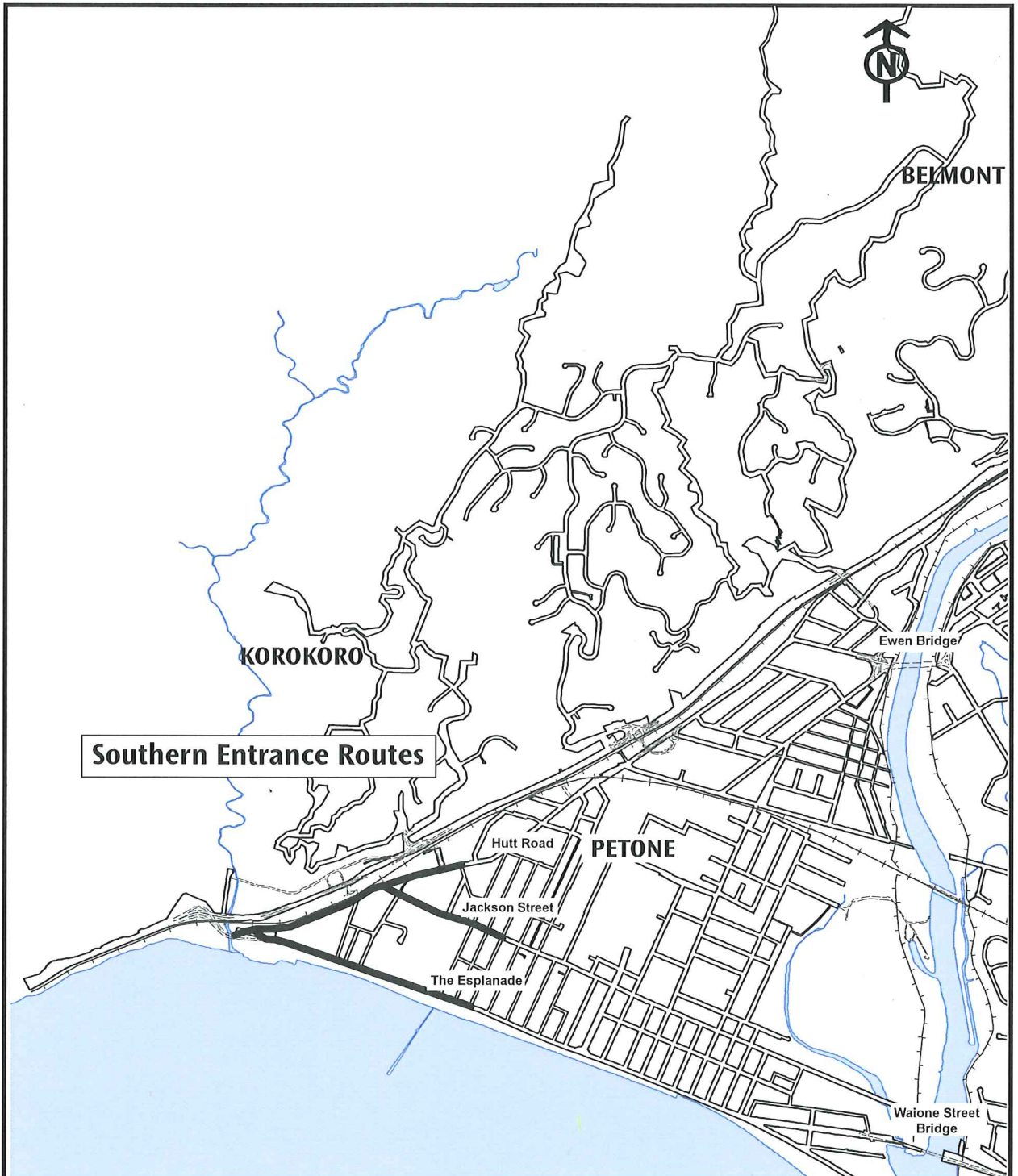




Appendix Petone Commercial 3 – Main Entrance Routes  
which pass through Petone Commercial Activity Area 2 –  
Petone Mixed Use

Amendment 41

## Appendix Petone Commercial 3



**Main Entrance Routes which pass through  
Petone Commercial Activity Area 2 - Mixed Use**

# Appendix Petone Commercial 4 – Wind Report

Amendment 42

Buildings above 12 metres require a wind assessment report to identify and describe measures for addressing the potential adverse of wind on public space, including streets.

Typically headings for a wind assessment report would be:

- Existing wind conditions/environment.
- Existing built context and environment in terms of height and bulk of surrounding buildings.
- Location of the site relative to public spaces.
- Proposed building height and form.
- Design features proposed to manage wind effects.



# Appendix Petone Commercial 5 – Petone Commercial Activity Area 1

Amendment 4

# Appendix Petone Commercial 5



Petone Commercial Activity Area - Area 1

# Appendix Petone Commercial 6 – Petone Commercial Activity Area 2

Amendment 4

# Appendix Petone Commercial 6



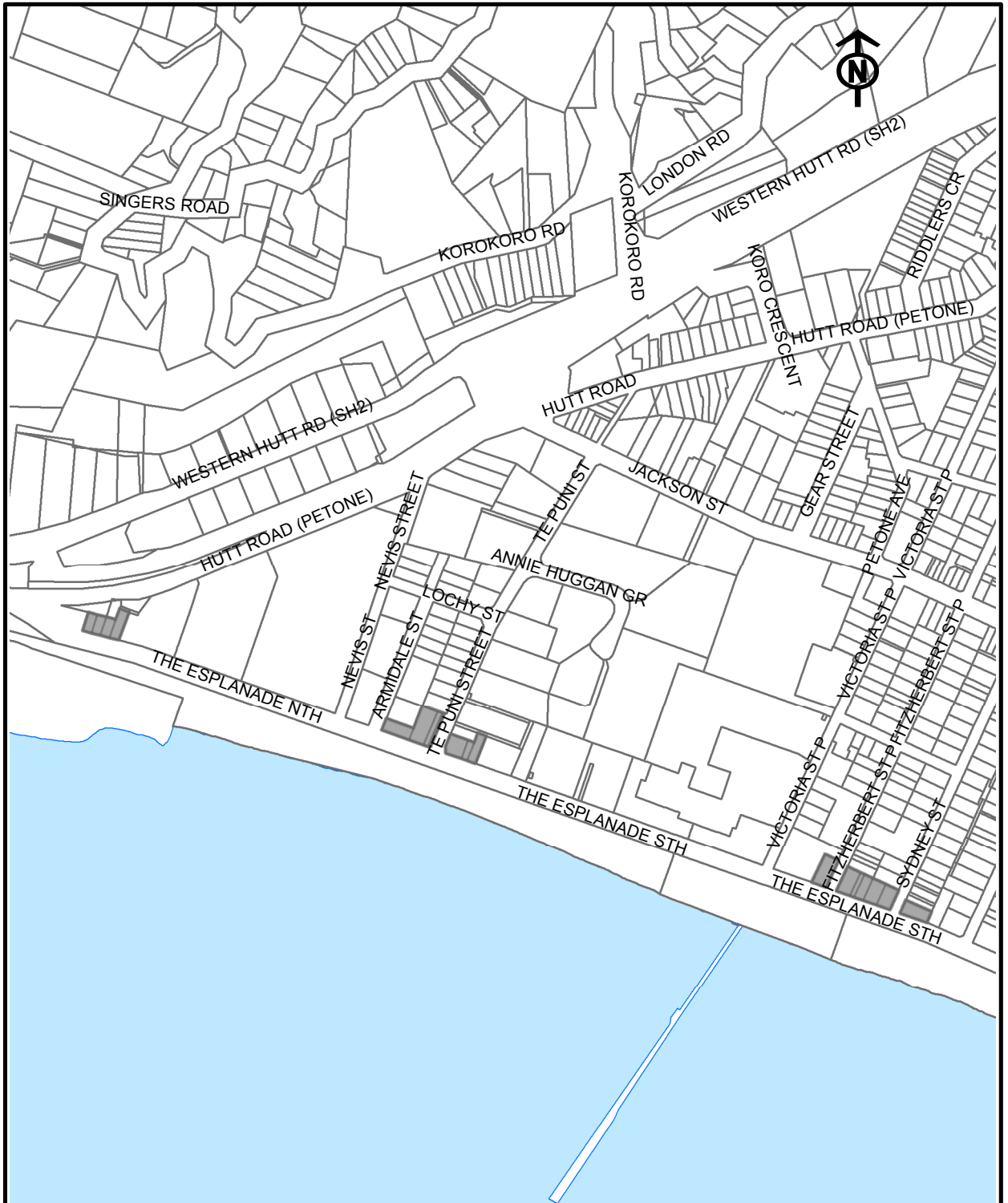
## Petone Commercial Activity Area - Area 2



# Appendix Petone Commercial 7 – Petone Commercial Activity Area 2

Amendment 11

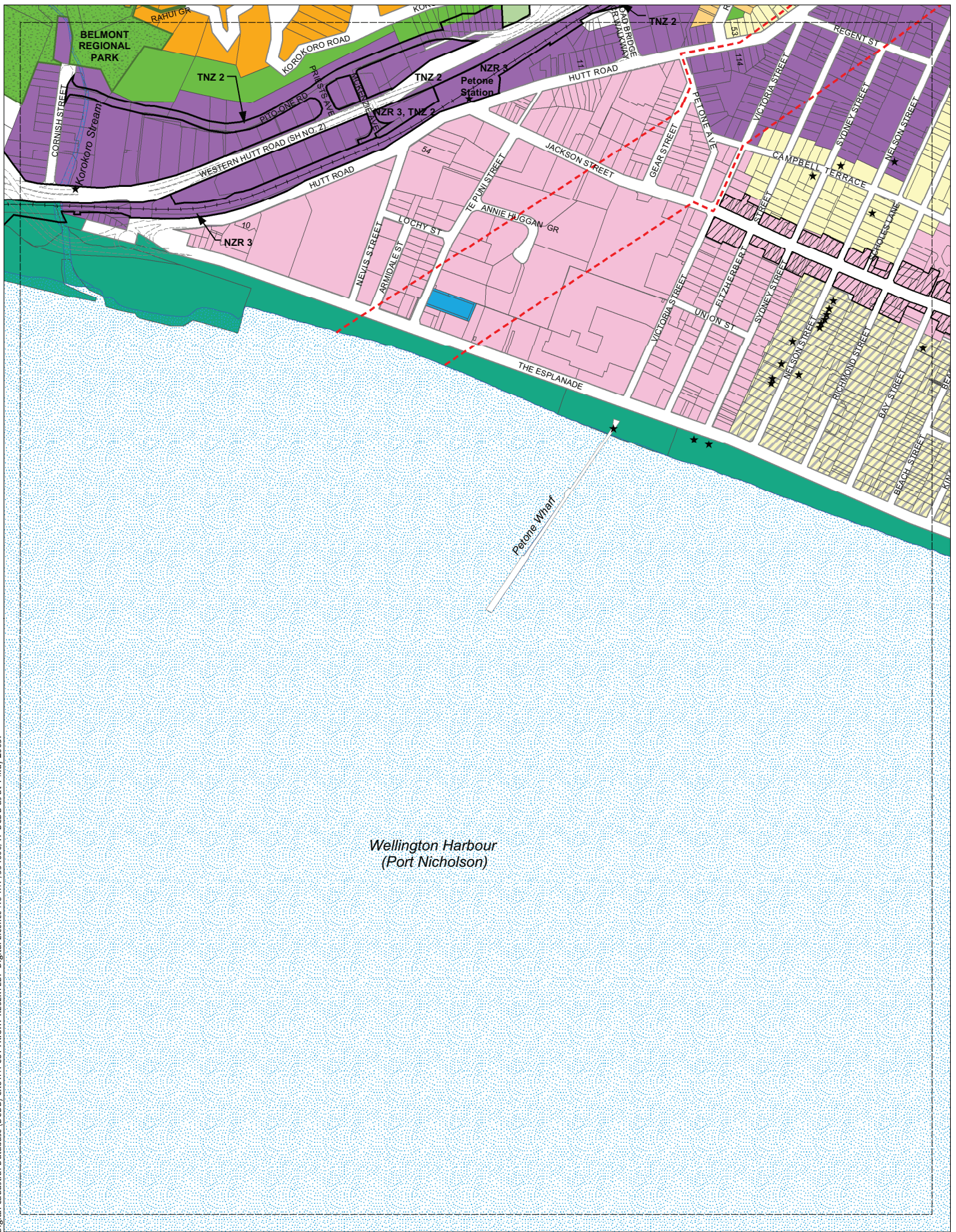
# Appendix Petone Commercial 7



Sites Exempt from 10 metre Setback  
from The Esplanade

# Planning Map A5

Amendment 43



Wellington Harbour  
(Port Nicholson)

Corrected 19 July 2012

**ACTIVITY AREAS**

- |                      |                     |                    |
|----------------------|---------------------|--------------------|
| General Residential  | Central Commercial  | Extraction         |
| Special Residential  | Petone Commercial   | General Recreation |
| Historic Residential | Suburban Commercial | Special Recreation |
| Hill Residential     | Special Commercial  | River Recreation   |
| Landscape Protection | General Business    | Passive Recreation |
| Rural Residential    | Special Business    | Community Health   |
| General Rural        | Avalon Business     | Community Iwi      |

**ANNOTATIONS**

- |                            |                            |
|----------------------------|----------------------------|
| City Boundary              | Regional/Forest Park       |
| Designation                | Heritage Area              |
| Medium Density Residential | Primary River Corridor     |
| Wellington Faultline       | Secondary River Corridor   |
| Special Study Area         | 1 in 100 Year Flood Extent |
| Hydraulic Line             | Building Setback Line      |
| Flood Protection Bank      | Notable Tree               |
| Railway Line               | Historic Place             |



A4	B4
[Map Area]	
B5	



## 6A 1 Issues, Objectives and Policies

### 6A 1.1 Local Area Issues

#### 6A 1.1.1 Accommodation of a Mix of Activities

##### Issue

**A mix of activities should be accommodated where the potential effects of non industrial activities are not dissimilar to those generated by industrial activities. In accommodating non industrial activities, it is necessary to avoid or mitigate any adverse effects on the amenity values of the area and the environment.**

##### Objective

To accommodate those non industrial activities which are suitable in the General Business Activity Area and which do not cause adverse effects on amenity values of the area and the receiving environment.

##### Policies

- (a) Accommodate those commercial activities which provide a local service for the work force in the same area.
- (b) Accommodate commercial recreational activities which are appropriate within the General Business Activity Area and which are compatible with the surrounding area.
- (c) Accommodate certain retail activities which are compatible with other activities of the General Business Activity Area and do not undermine the strength, vitality and viability of commercial activity areas.
- (d) That the accommodation of non industrial activities avoids or mitigates adverse effects on the amenity values of the area and the environment.

##### Explanation and Reasons

A range of industrial and commercial activities are accommodated, based on controlling the adverse effects likely to be generated. The range of commercial activities accommodated should not, however, undermine the strength, vitality and viability of commercial centres. Therefore, certain retailing activities are permitted where they are not suitable in commercial centres due to their nature and character. For example, kit set garages, caravans, trailers and boats. Natural materials are included where they are sold in bulk, such as gravel, shingle, rock, concrete, coal, firewood and timber.

The accommodation of commercial recreation activities includes indoor sports complexes, go-kart racing and tenpin bowling alleys.

Those commercial activities which provide a local service for the work force are also accommodated. These activities include restaurants, dairies, takeaway bars, day care centres and creches. Kokiri centres are also permitted as the likely generated effects will be similar to those generated by business activities.

The range of non industrial activities accommodated also includes training facilities, conference centres, places of assembly and places of worship. These activities are

provided for where the potential generated effects do not have an adverse effect on the amenity values of the area and the environment.

## 6A 1.1.2

Amendment 44

## Main Entrance Routes

### Issue

**The main entrance routes of the City need to be attractive and clearly defined to reinforce the identity of the City and the sense of place that it offers. Some of the main transport routes pass through the General Business Activity Areas. These routes include Railway Avenue, Hutt Road, Waione Street, and State Highway No.2. It is necessary to manage the effects of development along these routes to maintain and enhance the image of the City.**

### Policies

- (a) That the design, appearance and scale of structures or buildings fronting main transport routes be managed to emphasise the importance of the area as a major access to the City.
- (b) That all structures fronting main entrance routes are modulated and do not have continuous blank walls.
- (c) That the design and appearance of sites fronting main transport routes be maintained to avoid adverse effects on the visual quality of the routes and the surrounding area.
- (d) That the location, design and appearance of structures be managed to reinforce the definition of the street edge and enclosure of the sites fronting Waione Road.
- (e) That the effects of activities fronting Railway Avenue, Hutt Road, Waione Street, and State Highway No.2 be managed to enhance the image of these main entrance routes and avoid adverse effects on the amenity value and character of the surrounding areas.

### Explanation and Reasons

The objective is to maintain and enhance the image and visual appearance of main entrance routes within the City. These routes can enhance the experience of entrance to and exit from the City, thereby creating a sense of approach, arrival and departure as people travel through the City. This helps to reinforce the identity of the City and accentuates the sense of movement from one place to another when entering or leaving the City. This approach to the main entrance routes can assist peoples' sense of orientation and understanding of the physical structure, and the geography, of the City.

The main entrance routes include Railway Avenue, Hutt Road, Waione Street, and State Highway No. 2 identified in Appendix General Business 1.

#### (a) **Railway Avenue:**

Railway Avenue is an important point of entry into Hutt City's Central Commercial Activity Area. The activities fronting Railway Avenue should reflect such locational importance and enhance the image of this main entrance route. This area accommodates industrial and commercial activities, which provide a supporting role to the Hutt City Central Commercial Activity Area. These activities should attain high levels of amenity and aesthetic standards to emphasise the importance of this entrance route.

**(b) Hutt Road:**

The role of Lower Hutt as a regional industrial and service centre is emphasised by the character of the buildings and type of activities along this route. There are significant features along this route that mark progress. These include the residential area at Riddlers Crescent and the railway overbridge. This route provides a link between the Petone Off Ramp and the Hutt City Central Commercial Activity Area. Activities along this route should enhance the amenity value and visual appearance of this main entrance route.

**(c) Waione Street:**

Waione Street carries a significant volume of traffic along the Petone foreshore area. This route is at the edge of the City and is part of the connection with the harbour, which is one of the region's most significant natural features. There is a contrast between, on the one side of the road, a building dominated townscape, and on the other, a panoramic seascape.

There is a transition at Waione Street from a seaside cottage style residential area to an industrial area which has a built up nature. The distinct industrial character and diversity of form contributes to signalling progression along the route. This contrasts with the openness and long range views to both the central City, and the river and harbour that can be obtained from the Estuary Bridge.

The buildings located close to the street boundary provide a sense of enclosure which contrasts with the openness of The Esplanade and the Hutt River, and emphasises a sense of progression along this route.

The aim of the policies is to maintain the sense of enclosure and contrast with open panoramic sea views on other stretches of the entrance route. Buildings will be permitted up to the street boundary and there will be no requirement for front or side yards. Outdoor storage, parking and servicing will be accommodated in the rear yard.

The building design and appearance will be managed to enhance the amenity value of the area. Design features will be incorporated to prevent visual monotony and promote a strong visual connection with the street. There is no landscaping requirement, however, landscaping may be permitted where it enhances the character of the site.

**(d) State Highway No.2:**

The business areas at Cornish Street and Hebden Crescent are highly visible from State Highway No.2. The effects generated by the activities need to be managed to enhance the visual appearance, amenity value and contribution to the image of the City. This involves attention to building design and appearance, landscaping and screening, signage and traffic generation.

### 6A 1.1.3 Environmental Effects

#### Issue

**Business activities (commercial and industrial activities) have the potential to generate adverse effects on the amenity values of the area and neighbouring areas at the interface. These adverse effects include noise, dust, odour, glare, light spill and traffic. These activities can also have an adverse effect on the receiving environment in terms of air, water, and soil contamination, or damage to ecosystems. It is, therefore, necessary to manage such adverse effects to maintain and enhance the quality of the environment.**

## Objective

To avoid or mitigate adverse effects on the amenity values of the area and neighbouring areas, and the receiving environment.

## Policies

- (a) That effects likely to be generated by each activity, such as noise, dust, odour and traffic, are managed to avoid or mitigate adverse effects on the amenity values and character of both the General Business Activity Area and interface areas.
- (b) That effects likely to be generated by each activity are managed to avoid or mitigate any adverse effects causing harm or damage to the receiving environment.

## Explanation and Reasons

The effects of activities in the General Business Activity Area on the environment may have an adverse impact on the character or amenity values of an area, or on the receiving environment.

These effects need to be managed to maintain and enhance the amenity values of the General Business Activity Area and their respective interface areas. Appropriate conditions are required at the interface areas to ensure that amenity values of neighbouring activity areas are not adversely affected.

The objective and policies promote protection of the receiving environment and enhancement of a good working environment that is attractive and healthy for present and future generations. The protection of the environment from harm or irreversible damage is important for the quality of land, air and water and the integrity of the ecological system.

## 6A 1.2 Site Development Issues

### 6A 1.2.1 Effects on the Amenity Values of the Area

#### Issue

**The sites, structures and buildings used by business activities (commercial and industrial activities) have the potential to generate adverse effects on the amenity values of the area and neighbouring areas at the interface. These adverse effects include out of scale development, poor site maintenance, litter, dust, and visual detracting. It is necessary to manage such adverse effects to maintain and enhance the amenity values of the area.**

#### Objective

To maintain and enhance the amenity values of the activity area and neighbouring areas.

#### Policies

- (a) That each site, structure and building is designed and maintained to enhance the amenity values and character of both the General Business Activity Area and adjacent activity areas.



- (b) That identified urupa sites be protected from inappropriate development on neighbouring sites.
- (c) That buildings or structures in the Point Howard General Business Activity Area located on Eastern Bays Marine Drive adjacent to the Seaview Tanker Terminal be designed and maintained to maintain and enhance the amenity values and character of the coastal environment.

### Explanation and Reasons

The objective and policies promote the enhancement of a good working environment that is attractive and healthy for present and future generations. The design and maintenance of sites, structures and buildings shall be managed to avoid or mitigate adverse effects on the character and amenity values of the area, and the respective interface areas. This involves attention being given to the design and external appearance of buildings and structures, and to the maintenance of the site. This also includes setback requirements for sites abutting urupa sites and standards and terms for development at the Point Howard General Business Activity Area to enhance the amenity values of the coastal environment.

## 6A 1.2.2

### Effects of the Hutt River Flood Hazard

Amendment 45

#### Issue

**Buildings and structures within the Primary or Secondary River Corridor of the Hutt River are subject to flood hazard effects and can also have adverse effects on flood protection structures. The size, scale and location of buildings and structures need to be managed to avoid or mitigate these adverse effects.**

**Areas not protected by flood protection structures are at risk of flooding by the Hutt River. The site at 61 Connolly Street (Lot 1 DP 87322 C.T.WN 54D/764) has in place flood protection measures and these measures need to be recognised and maintained to ensure flood hazard effects are properly managed.**

#### Objectives

- (a) To avoid or mitigate adverse flood hazard effects on buildings and structures.
- (b) To avoid or mitigate adverse flood hazard effects on flood protection structures.

#### Policies

- (a) To discourage the siting of buildings and structures in the Primary and Secondary River Corridors.
- (b) To ensure that buildings and structures in the Primary or Secondary River Corridor of the Hutt River have no more than minor adverse effects on flood protection structures.
- (c) To mitigate the effects of flood hazards on buildings and structures in the Primary and Secondary River Corridors by managing their location, size and scale.
- (d) Flood protection measures at 61 Connolly Street (Lot 1 DP 87322 C.T.WN 54D/764) need to be established and maintained to ensure buildings and structures on site are protected from adverse flood hazard effects of the Hutt River.

## Explanation and Reasons

Buildings and structures which are inappropriately located can have adverse effects on adjoining activities. Buildings and structures in the Primary or Secondary River Corridor of the Hutt River could adversely affect flood protection structures. Buildings and structures in the river corridor will also be subject to effects of fast flowing water, deep flooding and erosion. The outcomes identified in the Hutt River Floodplain Management Plan are relevant and should be taken into account in any assessment of effects. It is important that buildings and structures are discouraged in the Primary and Secondary River Corridors. It is therefore appropriate to control the location of buildings and structures.

The site at 61 Connolly Street (Lot 1 DP 87322 C.T.WN 54D/764) is within the Hutt River floodway as shown in the Hutt River Floodplain Management Plan. The site is currently well developed and has in place flood protection measures to avoid and mitigate the adverse flood hazard effects from the Hutt River. These measures need to be maintained so that the risk of flooding is minimal. Flood protection measures protecting the site to 10.1m above mean sea level (which is the 100-year flood level) will ensure that Hutt River flooding is mitigated.

In order to ensure that flood hazard effects are managed, minimum conditions are specified.

## 6A 2 Rules

### 6A 2.1 Permitted Activities

- (a) Activities which meet the conditions for Permitted Activities and are not included as a Controlled, Restricted Discretionary, Discretionary or Non-Complying Activity.

#### 6A 2.1.1 Permitted Activities - Conditions

(a) **Minimum Yard Requirements:**

- (i) The side yard shall be a minimum depth of 3m where the site abuts a residential activity area. No side yard is required where a service lane is provided.
- (ii) The rear yard shall be a minimum depth of 8m where the site abuts a residential activity area. This may be reduced if there is a service lane to the rear of the site and sufficient provision has been made for loading and unloading operations.

(b) **Setback Requirements:**

Buildings and structures abutting an urupa shall have a minimum setback of 3m.

All new buildings and structures or additions in the Primary or Secondary River Corridor with a gross floor area of 20m<sup>2</sup> or less and with a setback of 20m or more from a flood protection structure.

(c) **Maximum Height of Buildings and Structures:**

Maximum height of all buildings and structures is 12m.

**(d) Recession Planes:**

All buildings or structures shall comply with the recession plane requirements of the abutting residential activity area.

**(e) Maximum Site Coverage:**

- (i) Maximum site coverage - 100% subject to compliance with yards and screening requirements and off street carparking, loading and unloading requirements.
- (ii) That portion of the site not covered shall be to the rear of the site, for servicing and parking provision.

**(f) Dust:**

- (i) All outside areas shall be sealed, surfaced, or managed appropriately so that there is no dust nuisance at or beyond the boundary of the site.
- (ii) All use, handling and storage of goods, raw materials and waste materials shall be undertaken in such a manner so that there is no dust nuisance at or beyond the boundary of the site.

**(g) Odour:**

All activities shall be carried out in such a manner so as to ensure that there is not an offensive odour or fumes at or beyond the site boundary.

**(h) Light Spill and Glare:**

- (i) Artificial light shall not result in added illuminance in excess of 8 lux measured at the window of a dwelling on a neighbouring site.
- (ii) All activities shall be undertaken so as to avoid all unreasonable light spill beyond the site boundary.
- (iii) All activities, buildings and structures shall avoid glare (light reflection) beyond the site boundary.

**(i) Vibration:**

All activities that cause vibration shall be managed to ensure that no vibration is discernible beyond the site boundary.

**(j) Retail Activities:**

There shall be no retail activities except in the following circumstances:

- (i) Where the goods are manufactured on site; provided that the retailing shall be ancillary to such manufacturing. For the purposes of this Section, manufacturing excludes those activities which comprise only the packing, labelling, sorting, mixing or assembling of pre-made products.
- (ii) Where the retail activity involves the sale of: carports, kitset garages and sheds; trailers; caravans; boats; swimming pools, spa pools, new and used motor cars; heavy commercial and agricultural vehicles; machinery; and the accessory parts which are essential to the operation of those goods.
- (iii) Where the goods are natural materials which are sold in bulk: gravel, sand, shingle, rock, concrete, coal, fire wood and timber for construction purposes.
- (iv) Where the retail activity is a building improvement centre on Lots 536 and 555, SO 22903 - Vogel Street/Hollands Crescent, Naenae.
- (v) Where the goods are sold at an auction, or garden centre.
- (vi) Where the retail activity provides a food service, provided the gross floor area does not exceed 200m<sup>2</sup>.

- (vii) Where the retail sale of goods is from a service station.
- (viii) Where the retail activity is a supermarket on Lot 2 DP 15329 and Sections 493, 537, 551 and 555 Hutt District - Cambridge Terrace/Vogel Street, Naenae.

**(k) Residential Activities:**

No residential activities are permitted except where it is necessary to provide living quarters for a caretaker on site.

Amendment 47

**(l) Visitor Accommodation:**

No visitor accommodation is permitted.

**(m) Storage Requirements:**

- (i) All activities shall be provided with an outdoor area for storage of goods, materials and waste products. This area shall be clearly defined at the time an application is made for development or change of use and occupancy and shall be clear of vehicle access, manoeuvring and parking areas.
- (ii) All outdoor storage areas shall be of such dimensions as to adequately provide for the storage requirements of the proposed use.
- (iii) Waste management: All storage and disposal of refuse will be carried out in such a manner so as to avoid causing adverse effects beyond the boundary of the site.

**(n) Landscaping and Screening Requirements:**

- (i) All outdoor storage and servicing areas must be screened so that they are not visible from a road or public space. Where this is not practicable such areas must be screened by a close-boarded fence or a fence made of solid material with a minimum height of 1.8m.
- (ii) Where a site abuts a residential or recreation activity area, all outdoor storage and servicing areas must be screened by a close-boarded fence or a fence made of solid material with a minimum height of 1.2m and a maximum height of 1.8m.
- (iii) Where there are 5 or more parking spaces on site, that area is to be screened from the street and adjoining properties by a fence or wall not less than 1.5m in height.

**(o) Servicing Hours:**

Where a site abuts or is opposite a residential activity area, servicing of activities must not occur between the hours of 10.00pm and 7.00am, however this shall not apply to any activity on Pt Lot 1 DP 10694, Sec 1 SO 31984 and Secs 70, 71 and 72 Blk XIV Belmont SD where the hours of servicing are not limited (land adjacent to the Seaview Tanker Terminal).

**(p) 61 Connolly Street, Lot 1 DP 87322 C.T.WN 54D/764 (identified in Appendix General Business 4):**

In addition to the other Permitted Activity Conditions, the following shall apply to the scheduled activities on this site:

- (i) The site must be protected from flooding by flood protection measures. The flood protection measures protecting the site from Hutt River flooding must be to a minimum level of 10.1m above mean sea level; and
- (ii) All buildings and structures must have a minimum floor level of 9.2m above mean sea level; and
- (iii) There must be at all times an operational mobile onsite water pump that will be used to manage surface water run-off.

(q) **General Rules:**

Compliance with all matters in the General Rules - see Chapter 14.

## 6A 2.2 Controlled Activities

Amendment 48

- (a) Any Permitted Activity in the western area of Petone which does not comply with the Parking, Loading and Unloading Conditions for Permitted Activities in Chapter 14A. (The western area of Petone are identified in Appendix General Business 3.)
- (b) Any Permitted Activity on a site abutting or on the opposite side of a road from a residential activity area.

### 6A 2.2.1 Matters in which Council has Restricted its Discretion and Standards and Terms

Amendment 48

- (a) **Any Permitted Activity in the western area of Petone which does not comply with the Parking, Loading and Unloading Conditions for Permitted Activities in Chapter 14A.**
  - (i) The provision of adequate car parking. See section 14A(iii) 2.2.1 (Chapter 14A Transport) for car parking assessment matters.
  - (ii) The provision for loading and unloading facilities. See section 14A(iv) 2.2.1 (Chapter 14A Transport) for loading and unloading provisions.
- (b) **Any Permitted Activity on a site abutting or on the opposite side of a road from a residential activity area.**
  - (i) External appearance and design of buildings and structures:  
All buildings and structures erected shall be of such design and appearance to maintain and enhance the amenity values of the adjoining area.
  - (ii) Maintenance of structures, buildings and space about buildings:  
All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.
  - (iii) Traffic generation:  
The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.

### 6A 2.2.2 Other Matters

All Controlled Activities must comply with the other relevant Permitted Activity Conditions.

## 6A 2.3 Restricted Discretionary Activities

- (a) Any activity which involves vehicle repairs, vehicle maintenance and servicing, engine and bodywork, spray painting and panel beating, or car wrecking and is located, either wholly or in part on a site abutting or on the opposite side of a road from a residential activity area.
- (b) Any building or structure on a site fronting Waione Street.

- (c) Any building or structure on a site fronting Railway Avenue.
- (d) Any building or structure on a site fronting Hebden Crescent.
- (e) Any building or structure on a site fronting State Highway No.2 and the Hutt Road.  
(These main entrance routes are identified in Appendix General Business 1.)
- (f) Any building or structure on Pt 1 DP 10694, Sec 1 SO 31984 and Sec's 70, 71 and 72 Block XIV Belmont Survey District - land adjacent to Seaview Tanker Terminal.
- (g) All new buildings and structures or additions in the Primary or Secondary River Corridor with a gross floor area greater than 20m<sup>2</sup> or with a setback less than 20m from a flood protection structure.

### 6A 2.3.1 Matters in which Council has Restricted its Discretion and Standards and Terms

- (a) **Any activity which involves vehicle repairs, vehicle maintenance and servicing, engine and bodywork, spray painting and panel beating, or car wrecking and is located, either wholly or in part on a site abutting or on the opposite side of a road from a residential activity area.**
  - (i) External appearance and design of buildings and structures:  
All buildings and structures erected shall be of such design and appearance to maintain and enhance the amenity values of the adjoining area.
  - (ii) Maintenance of structures, buildings and space about buildings:
    - The site shall be managed to maintain and enhance the amenity values of the adjoining area.
    - All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.
    - No vehicles shall be repaired in the front yard; nor shall vehicles in various stages of repair be parked in the street.
  - (iii) Traffic generation:  
The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.
- (b) **Any building or structure on a site fronting Waione Street.**
  - (i) Sense of enclosure along Waione Street:  
Buildings are to be built up to the front boundary.
  - (ii) External appearance and design of buildings and structures:
    - All buildings and structures shall be of such design and appearance to maintain and enhance the amenity values of the adjoining area, and the image of the main entrance route.
    - All buildings and structures shall be designed and constructed such that the facade nearest to the street incorporates design features that prevent visual monotony and promote a strong visual connection with the street.
    - Provision for parking, servicing and outdoor storage to be made in the rear yard. No parking in front yards.

- (iii) Maintenance of structures, buildings and space about buildings:  
All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.

- (iv) Traffic generation:  
The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.

Amendment 49

**(c) Any building or structure on a site fronting Railway Avenue**

- (i) Design and location of buildings and structures:
- All buildings and structures erected shall be of such design and appearance to maintain and enhance the amenity value of the area and the image of Railway Avenue as a main entrance route.
  - All buildings and structures shall be designed and constructed such that the facade nearest to the street incorporates design features that prevent visual monotony and promote a strong visual connection with the street.
  - The maximum building facade width of a building, structure or the primary forms of any multi-unit building at the street edge shall not be out of keeping with the local characteristic building width.
  - The publicly relevant activities such as entrance areas, windows, display areas, and offices within all buildings shall be located immediately adjacent to the street edge.
  - All sites, structures and buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity value of the area.
  - There shall be no front yard or required setback of buildings from the front boundary and buildings should be located as close as is practicable to the boundary with Railway Avenue.
  - Where it is proposed to have the building set back from Railway Avenue, the assessment will consider the following factors:  
Building design and appearance;  
The location of landscape planting. Soft landscaping of the front yard adjacent to Railway Avenue, where the design contributes to the urban landscape of the area.  
The location of publicly relevant activities (entrance areas, windows, display areas, offices) as close as practicable to the street edge; and  
The location and finished appearance of carparking and access areas. Carparking areas and hard-paved storage areas should not be located in front yards in such a way as to separate buildings from the street edge.
- (ii) Landscaping requirements:  
Soft landscaping of the front yard adjacent to Railway Avenue should be minimised and permitted only where it contributes to the sense of enclosure along Railway Avenue.
- (iii) Car parking and servicing:
- Car parking areas shall not be provided in any yard area immediately between buildings and Railway Avenue, but should be located in side and rear yard areas.
  - Loading and unloading facilities should be provided in the rear or side yards of the site.

- (iv) Layout and location of activities and facilities not enclosed within a building or structure:

The site shall be designed and maintained in such a manner so as to enhance the amenity values of the area and the image of Railway Avenue as a main entrance route of the City.

- (v) Traffic generation:

The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.

**(d) Any building or structure on a site fronting Hebden Crescent.**

- (i) Design and appearance of buildings and structures:

Any building or structure erected shall be of such design and appearance to maintain and enhance the amenity values of the area and the image of State Highway No.2 as a main entrance route.

- (ii) Maintenance of structures, buildings and space about buildings:

All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.

- (iii) Maximum height of buildings and structures: 8 metres.

- (iv) Landscaping requirements:

A comprehensive landscaping plan shall be prepared, showing not less than 15% of the site area to be landscaped. Landscaping shall be carried out in terms of the approved plan before any activity commences on the site.

- (v) Maximum site coverage:

Maximum site coverage 60% provided that the portion of the site not covered shall be nearest to Hebden Crescent, for parking, servicing and outdoor storage facilities.

- (vi) Yard requirements:

The rear yard shall be 1.5 metres to allow for the provision and protection of bulk services.

- (vii) Layout and location of activities and facilities not enclosed within a building or structure:

The site shall be designed and maintained in such a manner so as to enhance the amenity value of the area and the image of State Highway No.2 as a main entrance route of the City.

- (viii) Vehicular access:

All vehicular access shall be from Hebden Crescent.

- (ix) Traffic generation:

The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.

**(e) Any building or structure on a site fronting State Highway No.2 and the Hutt Road**

- (i) Design and appearance of buildings and structures:

Any building or structure erected shall be of such design and appearance to maintain and enhance the amenity value of the area and the image of



the route as a main entrance route. This shall be assessed in terms of design, shape, bulk, height and location of all buildings and structures.

- (ii) Maintenance of structures, buildings and space about buildings:

All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.

- (iii) Layout and location of activities and facilities not enclosed within a building or structure:

The site shall be designed and maintained in such a manner so as to enhance the amenity value of the area and the image of the route as a main entrance route of the City.

- (iv) Traffic generation:

The safe and efficient movement of all traffic needs to be ensured. It should be demonstrated that traffic generation and vehicles entering and leaving the site will not adversely affect the traffic flows on the road, or cause a traffic hazard.

- (f) Any building or structure on Pt 1 DP 10694, Sec 1 SO 31984 and Sec's 70, 71 and 72 Block XIV Belmont Survey District - land adjacent to Seaview Tanker Terminal.**

- (i) Design, appearance and location of buildings and structures:

Any building or structure erected shall be of such design and appearance to maintain and enhance the amenity values of the area. This shall be assessed in terms of design, shape, bulk, height and location of all buildings and structures.

- (ii) Maintenance of structures, buildings and space about buildings:

All structures, buildings and space about buildings shall be maintained in a tidy state at all times to ensure that there is no adverse effect on the amenity values of the surrounding area.

- (iii) Maximum height of buildings and structures: 8 metres.

- (iv) Landscaping:

A comprehensive landscaping plan shall be prepared, showing not less than 15% of the site area to be landscaped. Landscaping shall be carried out in terms of the approved plan before any activity commences on the site.

- (v) Maximum site coverage: 60%.

- (vi) Layout and location of activities and facilities not enclosed within a building or structure:

The site shall be designed and maintained in such a manner so as to enhance the amenity values of the area.

- (g) All new buildings and structures or additions in the Primary or Secondary River Corridor with a gross floor area greater than 20m<sup>2</sup> or with a setback less than 20m from a flood protection structure.**

- Proximity of buildings and structures to flood protection structures; and
- Adverse effects of the flood hazard on buildings and structures and on flood protection structures.

## 6A 2.3.2 Other Matters

All Restricted Discretionary Activities must comply with the other relevant Permitted Activity Conditions.

## 6A 2.4 Discretionary Activities

- (a) Except where stated in the General Rules, any Permitted, Controlled or Restricted Discretionary Activity which fails to comply with any of the relevant Permitted Activity Conditions, or relevant requirements of Chapter 14 - General Rules.
- (b) Any permitted retail activity identified in 6A 2.1.1 (j) from a site fronting or abutting State Highway No.2.
- (c) Residential activities, except where it is necessary to provide living quarters for a caretaker on site.
- (d) Visitor accommodation.
- (e) Waste transfer stations.
- (f) Any offensive trade specified in the Third Schedule to the Health Act 1956 and amendments.
- (g) Any activity which involves the use of radiation processes emitting up to 1,000,000 curies. This does not include x-ray machines and low level laboratory chemicals licensed by the National Radiation Laboratory, under the Radiation Protection Act 1965.
- (h) Trading warehouses.
- (i) Brothels and commercial sexual services on a site abutting or directly across the road from schools, pre-school facilities, churches and other similar religious establishments or a residential activity area.

Amendment 50

### 6A 2.4.1 Assessment Matters for Discretionary Activities

- (a) The matters contained in sections 104 and 105, and in Part II of the Act shall apply.
- (b) The degree of compliance or non-compliance with any relevant Permitted Activity Conditions.
- (c) In addition to the above the following matters will be taken into account:
  - (i) In the case of any proposed retail activity fronting or abutting State Highway No. 2, the following matters will be assessed:
    - Whether access is provided from a service road;
    - Whether the activity is a low trip generating activity;
    - Whether the activity will cause an adverse effect on safety and efficiency of the through route. These adverse effects include an increase in traffic generation, parking demand and turning movements at each property entrance. Such adverse effects could create a conflict with Transit New Zealand's strategy for state highway networks; and
    - Whether there is an adverse effect on the amenity values of the area, the strength of commercial activity areas, and the quality of the environment.
  - (ii) For activities involved with radiation processes the assessment shall consider the level of protection for workers, the general public and adjacent premises from exposure above certain acceptable criteria, under the Radiation Protection Act 1965.

## 6A 2.5 Non-Complying Activities

Amendment 51

- (a) Any retail activity which is not identified as a Permitted Activity.
- (b) Any activity involving the use of radiation in excess of 1,000,000 curies or acceptable limits as defined in the Radiation Protection Act 1965 and subsequent amendments.
- (c) Any abattoir, slaughterhouse or packing house required to be licensed pursuant to the Meat Act 1981 and amendments.

## 6A 2.6 Other Provisions

- (a) Subdivisions - See Chapter 11.
- (b) Financial Contributions - See Chapter 12.
- (c) Utilities - See Chapter 13.
- (d) General Rules - See Chapter 14.

## 6A 3 Anticipated Environmental Results

- (a) Protection of other activity areas from the intrusion of industrial development, which has the potential to generate adverse effects on the community and environment.
- (b) Mitigation and containment within the site of adverse effects to maintain and enhance compatibility with adjacent sites, amenity values of the area, and the receiving environment.
- (c) Safeguarding of the life supporting capacity of air, water, soil and ecosystems of the area from harm or irreversible damage.
- (d) Protection of the community and the environment from potential adverse effects associated with the use, storage, transportation and disposal of hazardous substances.
- (e) Accommodation of a mix of both industrial and non industrial activities.
- (f) The scale, size and location of buildings and structures will have adverse effects which are no more than minor on flood protection structures.
- (g) The adverse effects on buildings and structures in the Primary and Secondary River Corridors will be avoided or mitigated.

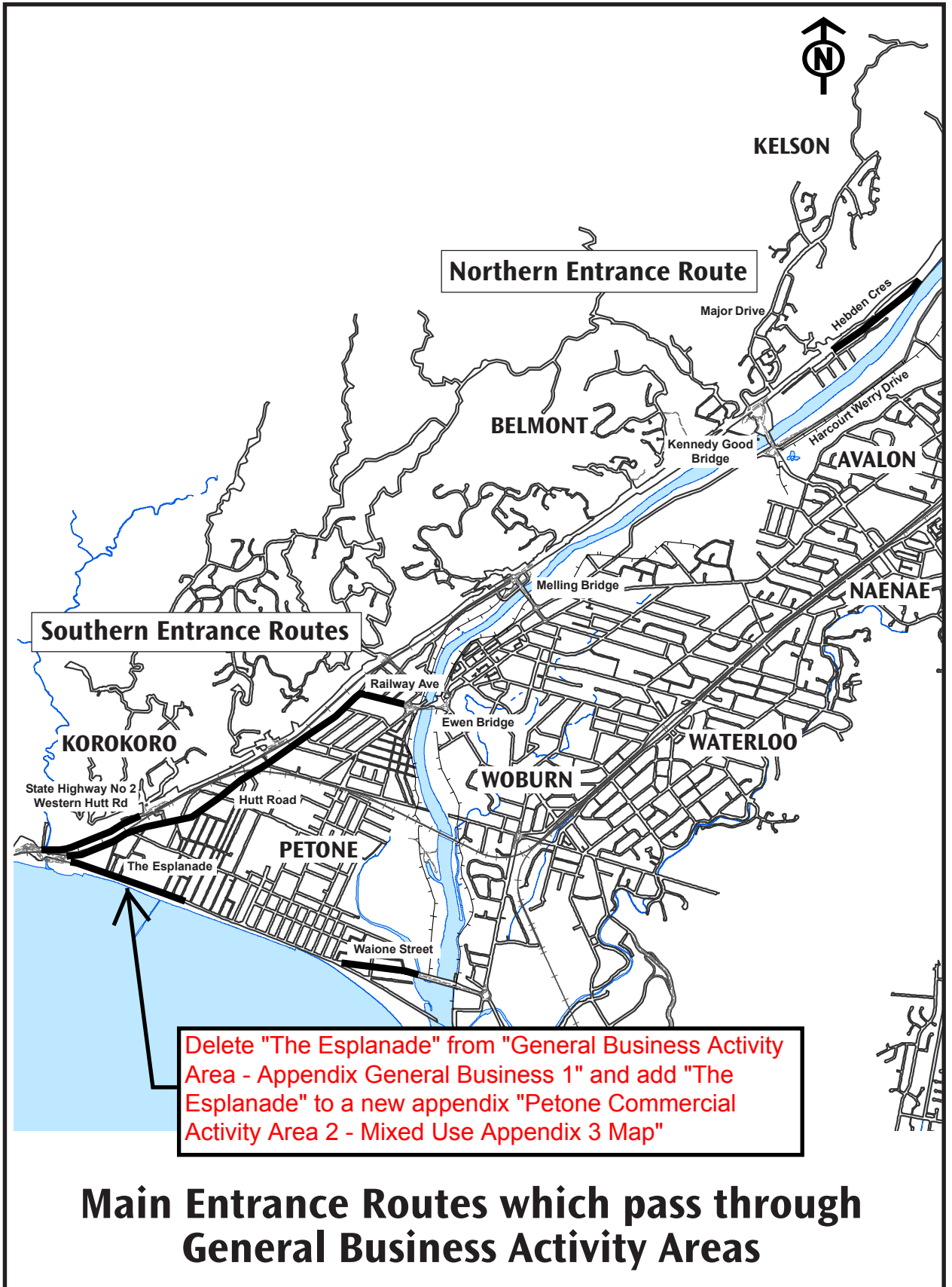


## Appendix General Business 1 – Main Entrance Routes which pass through General Business Activity Areas

Amendment 52

Amendment 53

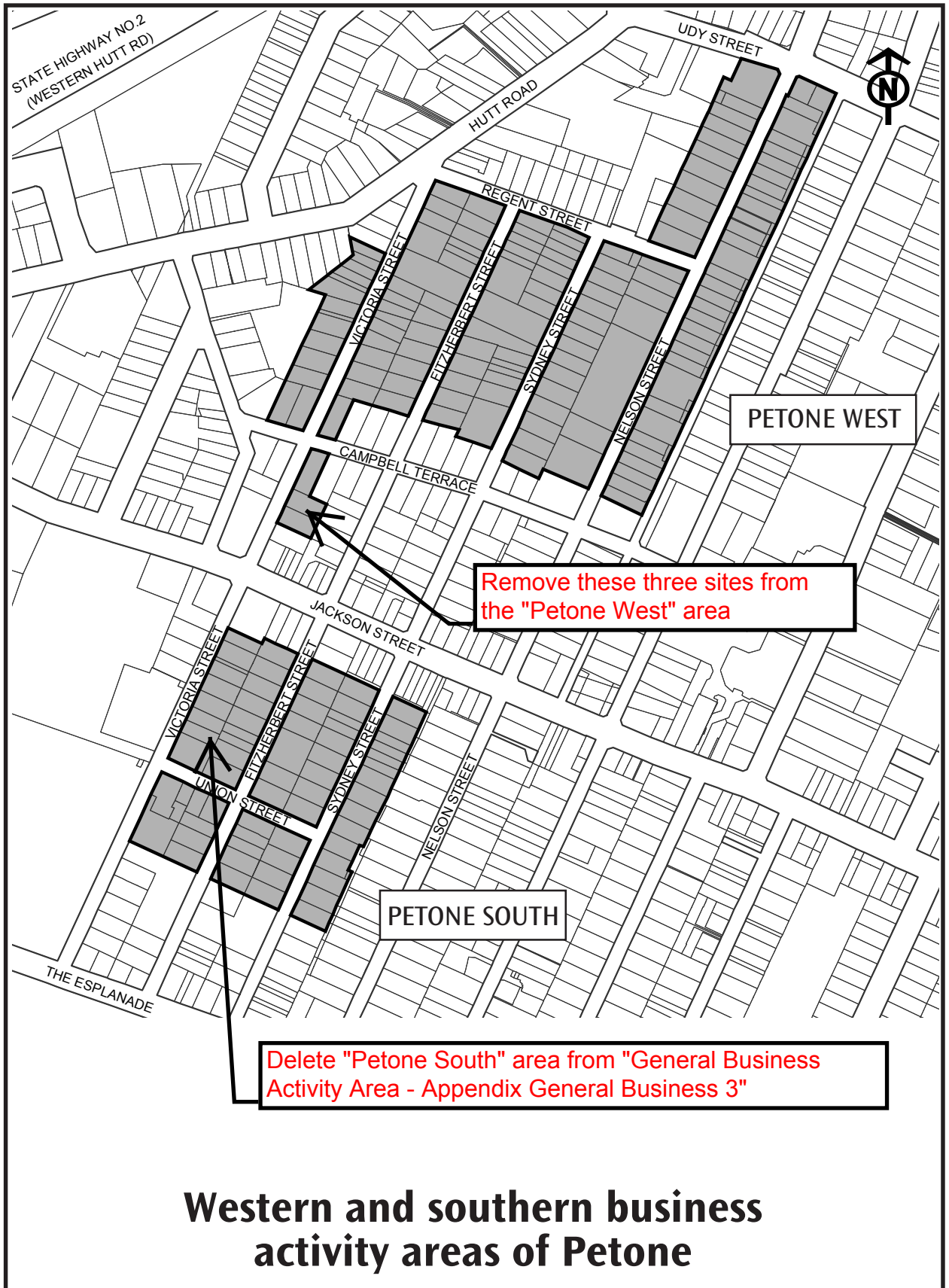
# Appendix General Business 1



# Appendix General Business 2 – Western Business Activity Area of Petone

Amendment 54

### Appendix General Business 3





# 14A(iii) Car and Cycle Parking

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## 14A(iii) 1 Issues, Objectives and Policies

### 14A(iii) 1.1 Local Area Issues

#### 14A(iii) 1.1.1 Adequate Car Parking Provision in the Hutt City Central Commercial Activity Area

##### Issue

The increased ownership of private vehicles and increased activity in the Hutt City Central Commercial Activity Area in recent years has contributed to a high demand for long and short stay parking. Each activity should provide sufficient parking on site, however, the inner area sites are generally small which makes it difficult to provide on site parking. It is also desirable to maintain a continuous pedestrian frontage for shoppers.

Policies for the Hutt City Central Commercial Activity Area have maintained the approach that sites within the inner area are not generally required to provide on site parking, though parking for residential units are required. Sites in the outer area will be required to provide on site parking to meet the high demand for long and short stay parking. The provision of adequate and well located parking contributes to the maintenance of amenity values, and the vitality and viability of the Central Commercial Activity Area.

##### Objective

To maintain the safety and amenity values, and support the vitality and viability of the Hutt City Central Commercial Activity Area through the provision of sufficient long and short stay car parking.

##### Policies

- (a) That sufficient long and short stay off street parking be provided in the vicinity of the Inner Central Area Parking District to enhance the safe and efficient operation of the roading system.
- (b) That sufficient long and short stay on site parking be provided in the Outer Central Area Parking District to enhance the safe and efficient operation of the roading system.
- (c) That sufficient parking spaces be provided using a graduated scale for retailing activities, commercial services and licensed premises.
- (d) That adequate on site car parking is provided for residential activities in the Hutt City Central Commercial Activity Area, recognising the different character and amenity between the Inner and Outer Central Area Parking Districts.

## Explanation and Reasons

The provision of sufficient on and off street parking for the inner area has been maintained over a number of years. The inner area is identified as the Inner Central Area Parking District and is shown in Appendix Transport 2. Sites in this area shall not be required to provide on site parking, as these sites are generally small, and it would degrade the overall quality of the central area, such as breaking up the continuous pedestrian shopping frontage. However, residential activities in the Inner Central Area Parking District are required to provide on site parking to meet the parking demand generated by this type of activity, while maintaining or enhancing the character and amenity of the central area.

Parking within the Inner Central Area Parking District has been provided through two main public parking areas. There is on street parking for short stay purposes and both long and short stay parking is available at the Riverbank Carpark and the Centre City Plaza Car Park. Changes may occur in the future to these existing parking areas, as well as the provision of new parking facilities in other locations. This ensures that there is adequate long and short stay parking available for the central area workforce and retailers.

The Outer Central Area Parking District does require that adequate on site parking provision is made. The parking requirements for retail activities will be determined on the gross floor area of the building, using a graduated system. Where a comprehensive retail development involves a range in sizes of retail activities then the parking requirement will be determined on an aggregate basis.

There are also on site car parking requirements for other activities. The on site parking requirement for each activity is based on the type and scale of activity, and the associated trip generation factor. The criteria have been based on appropriate measures of the intensity of each activity, such as gross floor area. The Outer Central Area Parking District is shown in Appendix Transport 2.

The provision of adequate car parking in the Hutt City Central Commercial Activity Area assists the safe and efficient operation of the roading system, thereby contributing to the vitality and viability of the commercial centre. However, large surface areas of car parking can detract from the streetscape and amenity values in the Hutt City Central Commercial Activity Area. In addition, the provision of parking must be considered in relation to travel demand and increased traffic movements, which can also decrease the amenity and attractiveness of the Hutt City Central Commercial Activity Area.

### 14A(iii) 1.1.2 Adequate Car Parking Provision in the Petone Commercial Activity Area

Amendment 55  
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#### Issue

**The availability of short stay parking in close proximity to the retail shops and small-scale commercial activities along Jackson Street is an important factor for business viability. There is a need for additional parking provision in the Petone Commercial Activity Area, particularly between Victoria and Cuba Streets. However, it is undesirable to provide access to off street parking along Jackson Street within Area 1, as this tends to break up the pedestrian frontage and the commercial properties lack depth.**

**If the on street parking is insufficient within Areas 1 and 2 of the Petone Commercial Activity Area, there is the potential for poor parking behaviour which creates a traffic hazard, visual detraction and an adverse impact on the amenity value of the area. Safe and adequate off street parking should be available in the vicinity of Jackson Street, both within Areas 1 and 2, to accommodate the parking demand of the workforce and shoppers.**

**Nevertheless, parking demands by workforce and shoppers need to be managed to prevent increases in traffic volumes on the wider road network from being generated by increased car parking provision. Providing a high level of on-site car parking, can encourage the use of private vehicles and discourage the use of more sustainable forms of transport.**

### Objective

To provide adequate car parking in a safe and visually attractive manner, to maintain the safety and amenity values of the area.

### Policies

- (a) That sufficient parking spaces be provided using a graduated scale for retailing activities and licenced premises.
- (b) That on site parking be provided in a safe and visually attractive manner to enhance the safe and efficient operation of the roading system.

### Explanation and Reasons

The objective and policies seek to ensure that the safety and efficiency of the road system is maintained, and that adverse effects on the amenity values or character of the area are mitigated.

The sites along Jackson Street within Area 1 are small and it is difficult to provide on site parking or rear service lanes. It is not necessary for small retail shops or commercial activities below 500m<sup>2</sup> to provide onsite parking as it is inappropriate to disrupt the commercial frontage. The needs of such small retail shops and commercial offices should be met by on street parking. However, larger retail premises would require on site parking provision, particularly where integrated retail development is proposed.

There are some sites in Area 2 – Petone Mixed Use which will find it difficult to provide on site parking and servicing areas in accordance with the permitted activity standards, where the site is small and there is a narrow road frontage. New development shall ensure that sufficient provision is made for parking, loading and unloading facilities.

On street parking will be controlled by a traffic management plan as necessary to avoid the adverse effects of poor parking behaviour on both safety and efficiency of the roading network.

The parking requirements for retail activities and licenced premises will be determined on the gross floor area of the building, using a graduated system. Where integrated retail development involves a range in sizes of retail activities then the parking requirement will be determined on an aggregate basis.

## 14A(iii)1.1.3 Adequate Car Parking Provision in the Suburban Commercial Activity Areas

### Issue

**There should be an adequate provision for long and short stay parking to meet the parking demand of the local workforce and shoppers. Such parking should not disrupt the continuous pedestrian shop frontage, nor create adverse effects on amenity values of the commercial centre.**

## Objective

To provide adequate car parking in a safe and visually attractive manner, to maintain the safety and amenity values of the area.

## Policies

- (a) That sufficient parking spaces be provided using a graduated scale for retailing activities, commercial services and licenced premises.
- (b) That on site parking be provided in a safe and visually attractive manner to enhance the safe and efficient operation of the roading system.
- (c) That on street parking be provided in a safe and visually attractive manner to enhance the safe and efficient operation of the roading system.

## Explanation and Reasons

The objective and policies seek to ensure that the safety and efficiency of the road system are maintained, and that adverse effects on amenity values or character of the area is mitigated. It is not necessary for small retail shops to provide on site parking as it is inappropriate to disrupt the commercial frontage. The needs of such small retail shops should be met by on street parking. However, larger retail premises would require on site parking provision, particularly where comprehensive developments are proposed.

On street parking will be controlled by the traffic management plan as necessary to avoid the adverse effects of poor parking behaviour on both safety and efficiency of the roading network.

The parking requirements for retail activities, commercial services and licenced premises will be determined on the gross floor area of the building, using a graduated system. Where a comprehensive retail development involves a range in sizes of retail activities then the parking requirement will be determined on an aggregate basis.

## 14A(iii) 1.1.4 Adequate Car Parking Provision in the Special Commercial Activity Areas

### Issue

**The Special Commercial Activity Area comprises Station Village and Boulcott Village. It is necessary to provide safe and adequate parking in these commercial centres to meet the needs for long and short stay parking purposes. Such parking provision should also maintain the amenity values of the area.**

### Objective

To maintain an adequate supply of parking spaces in a safe and attractive manner.

### Policies

- (a) That the existing supply of car parking spaces be maintained to meet the parking demand for long and short stay purposes.
- (b) That additional parking spaces be provided in a safe and visually attractive manner to enhance the safe and efficient operation of the roading system, and to maintain the amenity value of the area.

## Explanation and Reasons

The provision of sufficient parking is necessary to avoid an overspill of parking onto adjoining roads, which creates a traffic hazard, visual detraction and an adverse effect on the amenity values of the area. The present supply of car parking spaces must be maintained to meet the demand for long and short stay parking purposes.

Any additional parking spaces should be provided in a safe and attractive manner to maintain the safe and efficient operation of the roading network and maintain the amenity values of the area.

### 14A(iii) 1.1.5 Adequate Car Parking Provision in the Western Petone Business Activity Area

Amendment 56

#### Issue

**Many of the sites in the western area of Petone, which are in the General Business Activity Area, are generally small sites. Some of these sites are unable to provide sufficient space for parking and servicing. It is necessary to manage activities on these sites to ensure that adequate provision is made for car parking and servicing.**

#### Objective

To provide for adequate car parking and servicing in the western area of Petone in the General Business Activity Area.

#### Policy

- (a) That activities in the western area of Petone in the General Business Activity Area be controlled to ensure that adequate provision is made for car parking and servicing.

## Explanation and Reasons

There are some sites in the western area of Petone in the General Business Activity Areas which will find it difficult to provide on site parking and servicing areas, where the site is small and there is a narrow frontage. New development will be a controlled activity to ensure that sufficient provision is made for parking, loading and unloading facilities. On street parking may be used where this is appropriate.

### 14A(iii) 1.2 Site Development Issue

#### 14A(iii) 1.2.1 On Site Parking Provision For Activities

#### Issue

**The demand for parking is a generated effect of most activities. Poor parking behaviour on streets and footpaths in any activity area creates a traffic hazard, visual detraction and an adverse impact on the amenity values of the area. Attention needs to be given to the manner in which car parking is provided on site, as poor provision can have an adverse effect on the safe and efficient operation of the roading network, and on the amenity values of the area.**

To provide adequate on site car parking in a safe and visually attractive manner, to maintain the safety and efficiency of the roading system, and the amenity values of the area.

### Policy

- (a) That adequate on site parking space is provided for each type of activity in a safe and visually attractive manner.

### Explanation and Reasons

The objective and policy seek to ensure that the safety and efficiency of the road system are maintained, and that any adverse effects on the amenity values or character of an area are mitigated. The provision of adequate on site parking is an integral part of the safe and efficient operation of the roading system, linked strongly to both moving traffic and land use activities.

The demand for parking is a generated effect of most activities. Provision of sufficient parking is necessary to avoid overspill of parking onto the adjoining road and neighbouring properties. This situation creates a traffic hazard, visual detraction and an impact on the amenity values of the area. The policy requires that each activity provides sufficient parking spaces depending on their trip generation capacity and turnover characteristics.

The provision of numerous car parking spaces can have adverse effects on the amenity values of the area. Parking areas can create dust or mud if unsealed, and they can detract from the visual quality of the area. Attention to sealing, landscaping and screening will be required to reduce these adverse impacts.

## 14A(iii) 2 Rules

### 14A(iii) 2.1 Permitted Activities - Conditions

#### (a) Car Parking Requirements:

The following parking provisions shall apply in all activity areas where an activity is established on site; or there is a change in activity; or the building is constructed, substantially reconstructed, altered or added to. Except as provided for in this section and in Section (c) below (Special Parking Areas) and in relation to temporary activities and filming activities, all activities shall meet the minimum parking requirements set out in Appendix Transport 3.

Parking requirements are based on the type, scale and the associated trip generation factors for each activity. The minimum parking requirements are listed in Appendix Transport 3. Sufficient carparking shall be provided to meet the actual or 10th highest parking demand hour in any year during the life of the development. The parking standards in Appendix Transport 3 are deemed to meet this objective for parts of the City except as provided for in this section and in Section (c) below (Special Parking Areas).

In calculating the number of parking spaces to be provided, fractional numbers shall be rounded up to the next complete number.

If the proposed development will lead to the loss of public parking on an Access Road, either through additional length of kerb crossing, or through consequential parking prohibitions, then the number of parks lost shall be added to the requirement.

Space needed for manoeuvring, loading, unloading, queuing, or standing at a service booth, shall not be used for carparks counted towards meeting the requirement.

**(b) Location of Parking Spaces:**

Parking spaces must be provided on site.

**(c) Special Parking Area:**

**(i) Hutt City Central Commercial Activity Area -**

The provision of long and short stay parking is controlled by the provisions of the Inner and Outer Central Area Parking Districts.

Residential Activities: The minimum parking requirement for residential activities is 1 space for every two residential units.

Other Activities: No on-site parking requirements within the Inner Central Area Parking District.

Outer Central Area Parking District:

Retail Activities and Licensed Premises: A graduated system will be used to determine the appropriate amount of car parking spaces required. Where a comprehensive development involves a range in sizes of retail activities then the parking requirement will be determined on an aggregate basis. Where the inner/outer parking area boundary separates a comprehensive development, any carparking provided within the inner area can be used in the calculation of the total carparking requirement.

The minimum parking requirement for retail activities and licensed premises shall be based on the following graduated scale:

Less than 500 m<sup>2</sup> GFA - Nil

More than 500 m<sup>2</sup> GFA but less than 1500 m<sup>2</sup> GFA - 1 space per 100 m<sup>2</sup> GFA

More than 1500 m<sup>2</sup> GFA but less than 3000 m<sup>2</sup> GFA - 2 spaces per 100 m<sup>2</sup> GFA

More than 3000 m<sup>2</sup> GFA - 3 spaces per 100 m<sup>2</sup> GFA

Note: The above graduated system does not apply in a systematic way, in that the first 500m<sup>2</sup> of a 1800m<sup>2</sup> development is not exempt from the standard requiring 2 spaces per 100m<sup>2</sup>.

Residential Activities: The minimum parking requirement for residential activities is 1 space per single residential unit.

Other Activities: The minimum parking requirements for other Permitted Activities are listed in Appendix Transport 3.

Amendment 57

**(ii) Petone Commercial and Suburban Commercial Activity Areas -**

Residential Activities within Area 2 – Petone Mixed Use: The minimum parking requirement for residential activities is 1 space for every residential unit. Visitor parking is to be provided for residential developments for seven or more residential units, at the ratio of 1 visitor space for every four residential units.

Retail Activities and Licensed Premises: A graduated system will be used to determine the appropriate amount of car parking spaces required for retail activities and licenced premises. Where an integrated retail development involves a range in sizes of retail activities then the parking requirement will be determined on an aggregate basis.

The minimum parking requirement for retail activities and licenced premises shall be based on the following graduated scale:

Less than 500m<sup>2</sup> GFA - Nil

More than 500m<sup>2</sup> GFA but less than 1500m<sup>2</sup> GFA - 1 space per 100m<sup>2</sup> GFA

More than 1500m<sup>2</sup> GFA but less than 3000m<sup>2</sup> GFA - 2 spaces per 100m<sup>2</sup> GFA

More than 3000m<sup>2</sup> GFA but less than 5000m<sup>2</sup> GFA - 3 spaces per 100m<sup>2</sup> GFA

More than 5000m<sup>2</sup> GFA - 5 spaces per 100m<sup>2</sup> GFA

Note: The above graduated system does not apply in a systematic way, in that the first 500m<sup>2</sup> of a 1800m<sup>2</sup> development is not exempt from the standard requiring 2 spaces per 100m<sup>2</sup>.

Commercial activities up to 500m<sup>2</sup> within Petone Commercial Activity Area 1 – Jackson Street Historic Retail Precinct - Nil

Other Activities: The minimum parking requirements for other Permitted Activities are listed in Appendix Transport 3.

(iii) Special Commercial Activity Area -

Boulcott Village:

There shall be a minimum of 33 parking spaces maintained at Boulcott Village, comprising 23 on Lot 1 DP 69175, and 10 on Lot 2 DP 66201. Additional parking spaces shall be provided using the minimum parking requirements for Permitted Activities listed in Appendix Transport 3.

Station Village:

The existing supply of parking spaces shall be maintained. Additional parking spaces shall be provided using the minimum parking requirements for Permitted Activities listed in Appendix Transport 3.

**(d) Design Standards:**

The layout, design and detailing of all parking spaces shall be such as to ensure their convenient, safe and efficient use.

All parking shall be formed and maintained for use in all weathers. All parking shall be sealed or appropriately maintained at all times with a dust free surface. All parking spaces shall be kept clear at all times for the use of motor vehicles and shall not be used for the storage of goods, articles, materials or substances or for any other purpose.

Except where parking spaces are associated with a specific dwelling house, or for network utility operations it shall be possible to gain access to any space without shifting other vehicles.

Every parking space shall be provided with an access which is of a properly constructed motorcrossing; as is necessary for access of motor vehicles to and from a street. Sufficient space shall be allowed for vehicles to manoeuvre within the site. In determining the extent of area required for manoeuvring space, the provisions of Australian Standard AS2890 shall be taken into account.

Every parking space shall have such dimensions as to comply with the requirements of the Australian Standard AS2890 Part 1.



**(e) Cycle Parking Requirements:**

Space for the parking of bicycles shall be provided as follows:

For each building where there are 10 or more employees	Secure storage for cycles at the rate of 1 for every 30 employees.
At every place of assembly or footpaths entertainment facility:	Space for cycles to be placed clear of and roadways, not more than 50 metres from the public entrance, at the rate of 1 space for each 100m <sup>2</sup> GFA.

Stands need not be provided but storage must be safe with the possibility of securing cycles to an immovable object.

## 14A(iii) 2.2 Discretionary Activities

- (a)** Where a Permitted Activity proposes to provide less than the required number of parking spaces.
- (b)** Where a Permitted Activity is unable to provide the required number of parking spaces on site.
- (c)** Any other Permitted, Controlled or Restricted Discretionary Activity which fails to comply with any of the relevant Permitted Activity - Conditions.

### 14A(iii) 2.2.1 Assessment Matters for Discretionary Activities

- (a)** The matters contained in sections 104 and 105, and in Part II of the Act shall apply.
- (b)** The degree of compliance or non-compliance with any relevant Permitted Activity Conditions.
- (c)** In addition to the above the following matters will be taken into account:

**(i)** Provision of car parking spaces:

Where a Permitted Activity proposes to provide less than the required number of parking spaces, the proposal shall be assessed on the following matters:

- Whether the nature of the activity is such that the demand for parking will be permanently lower than the requirement.
- Whether the timing of the parking demand is such that it can be met by using car parks normally used by other activities.
- Whether sufficient public parking is available for use without causing significant adverse effects on other activities in the vicinity. The use of on street parking on Access Roads may be considered for this purpose.
- Whether there is reasonable public transport or access to the site, and a likelihood of it being used.

**(ii)** Location of parking spaces:

Where a Permitted Activity is unable to provide the required number of parking spaces on site, Council may approve spaces located elsewhere provided that:

- The fact that the spaces have been allocated to a different site is recorded as a Memorandum of Encumbrance on the title;

- Convenient pedestrian access between the development and the spaces is available and signposted;
- Parking shall be no more than 100 metres walking distance from doors of the development, except that this shall be reduced to 50 metres where it is necessary to cross a road, or ascend or descend a flight of steps more than 2 metres in height; and
- Pedestrians walking between the development and the spaces do not need to cross a road with a hierarchy classification higher than Access Road.

# 14A(iv) Loading and Unloading

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## 14A(iv) 1 Issue, Objective and Policies

### 14A(iv) 1.1 Site Development Issue

#### 14A(iv) 1.1.1 Safe and Adequate Provision for Servicing

##### Issue

**Inadequate or poor provision of servicing areas for loading and unloading purposes will create a traffic hazard, visual detraction and adverse effects on the amenity values of the area. Attention needs to be given to the manner in which loading and unloading facilities are provided on site to avoid adverse effects on the safety and efficiency of the roading network and on the amenity values of the area.**

##### Objective

To maintain the safety and efficiency of the roading network and the amenity values of the area.

##### Policy

- (a) That adequate on site loading and unloading provision be made in a safe and attractive manner.

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##### Explanation and Reasons

The objective and policy seek to ensure that the safety and efficiency of the road network is maintained, and that any adverse effects on the amenity values or character of an area are mitigated.

Loading and unloading facilities are an integral part of the road network, linked strongly to both moving traffic and land use activities. Adequate on site servicing provision is necessary to enhance the safety and efficiency of the roading network. Such provision shall be made in an attractive manner to maintain and enhance the amenity value of the area.

It is recognised that it is not always possible to provide sufficient on site loading and unloading facilities. For example, some of the business premises in the western area of Petone have a small site and narrow frontage, which makes it difficult to provide on site parking, loading and unloading facilities. New development will, therefore be a controlled activity to ensure that sufficient provision is made for parking, loading and unloading facilities. On street provision may be used where this is appropriate.

In the Hutt City Central Commercial Activity Area, the objective is to increase residential development and the standard loading requirements for larger scale residential development would limit the efficient use of this land.

# 14A(iv) 2 Rules

## 14A(iv) 2.1 Permitted Activities - Conditions

### (a) Loading and Unloading Requirements:

Every owner or occupier in all activity areas who constructs, substantially reconstructs or changes the use of a property, shall make adequate provision on the site for the loading of or unloading from vehicles of all goods and materials associated with the activity. No servicing activity may take place on road reserve.

For residential developments having fewer than 20 dwelling units or accommodating fewer than 20 residents, for all residential development in the Hutt City Central Commercial Activity Area, and for network utility operations on sites less than 200m<sup>2</sup> or for unstaffed network utility sites, the requirements of Chapter 14A - Transport, 14A(iii) Car Parking requirements are deemed sufficient.

### (b) Loading and Unloading Requirements for Non-Residential Activities:

For retail and industrial activities (except in the Core, Riverfront (Core), Riverfront (Commercial) and Residential Transition Precincts in the Hutt City Central Commercial Activity Area), the number of loading spaces to be provided shall not be less than the following requirements:

Table 7 - Loading Space Requirements:

Gross Floor Area	No. of Spaces	Minimum Design Vehicle
Under 2000m <sup>2</sup> GFA	1	Maximum rigid truck
2000 - 4000m <sup>2</sup> GFA	1	Maximum articulated truck Plus 1 Maximum rigid truck
more than 4000m <sup>2</sup> GFA	1	Maximum articulated truck Plus 2 Maximum rigid truck

For all other types of non-residential activity (including retail and industrial activities in the Hutt City Central Commercial Activity Area), one loading space shall be provided in accordance with the design requirements in 14A(iv) 2.1(c).

### (c) Design Requirements:

- (i) Loading spaces for light vehicles shall be a minimum of 6.0 metres long, 3.0 metres wide and 3.2 metres high.
- (ii) Loading platforms for heavy vehicles shall have a minimum width of 4.0 metres and minimum length of 8.5 metres, in addition to the space required for the vehicle. Where side loading or mechanical handling methods are proposed, additional space shall be provided in accordance with the particular methods and equipment contemplated.
- (iii) Where articulated vehicles are likely to be used, a greater depth shall be provided.

Design of all facilities, including access, manoeuvring and circulation space, shall take into account the provisions of Australian Standard AS2890 "Off-street Parking, Part 2: Commercial Vehicle Facilities (1989)."

## 14A(iv)2.2 Discretionary Activities

- (a) Any Permitted, Controlled or Restricted Discretionary Activity which fails to comply with any of the relevant Permitted Activity - Conditions.

### 14A(iv) 2.2.1 Assessment Matters for Discretionary Activities

- (a) The matters contained in sections 104 and 105, and in Part II of the Act shall apply.
- (b) The degree of compliance or non-compliance with any relevant Permitted Activity - Conditions.